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the

MainLine

magazine



the official journal of the

National Model Railroad Association Incorporated
Australasian Region

NMRA Inc - Australasian Region Directory

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All members of the Australasian Region are invited to submit articles of a railway nature for publication in the 'MainLine' magazine.

I would appreciate all articles to be sent to me in an editable format, such as 'Word, Pages, text, email, but not pdf, and high resolution photos sized up to 2MB in size.

Please send your articles to editor@nmra.org.au

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Articles are provided by members for publication in good faith and the views expressed therein are not necessarily those of the NMRA Inc - AR.

New Articles

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14	How to fit Building Lighting Lighting of model buildings can be lots of fun, and makes your scene look very realistic. In this article Steve Chapman MMR® outlines how he installs lighting in buildings and what pitfalls you may wish to avoid. by Steve Chapman, MMR®
16	A Weekend Project When modelling in the larger G scale, most accessories need to be scratchbuilt. Dave Whibley describes how he built a horse drawn transportation wagon for moving product from the distillery to the market on his layout. by Dave Whibley
19	CR&P Transfer Caboose A transfer Caboose is not something that is normally known about nor modelled, so Graeme Prideaux describes why you need one and how it build it. by Graeme Prideaux
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the Cover Photo

Rail motor set 622/722 arriving at 'Helper' station to pick up the early morning commuter passengers on Merv Bagnall's 'the Braidwood Division' layout.

Editor's Comments

The last couple of months has been an exciting and busy time in model railroading, with plenty happening in the divisions to report on.

There are 5 articles and 19 reports in this edition. The extra reports are not due to extra divisions being created, but is a result of some inadvertent problems occurring after the change-over to the NMRA-AR's new emailing system, which included how your editor received emails that were sent to the editor's address!


A number of divisional reports had floated around in cyberspace for a couple of months until all was rectified. Hopefully, I have now received all outstanding reports, and all are now up to date. If any Divisional Superintendent sees that one of their reports is still outstanding and not made it to the pages of MainLine yet, then please let me know.

Irrespective of these technical issues, I must extend congratulations to all the divisions and 100% clubs who are now well and truly in the swing of providing quality and regular reports covering the activities within their division or club. This complements the high quality articles which are also being received. I have no doubt that reading your regular submissions, inspires others in our hobby, and that is what I believe is the primary purpose for producing MainLine.

I must mention and give high praise to the organisers of the Armidale Convention, which was held in late October. Although not an NMRA-AR event, it was well supported by our members and I am confident that NMRA-AR members would have outnumbered non members. It was the first time I have attended this convention, but it certainly won't be the last time. It was well run, had an excellent partners program, plenty of clinic's, layouts and trade stands for us modellers to add to our knowledge and pick up a bargain or two! I was also able to meet a number of members in person with whom I have only known through phone calls or emails. It was also pleasing to receive positive feedback and enthusiasm for MainLine from a number of members. I have included a few photos of the event from page 81.

In this edition we start on page 6 with part 2 of Jeff Lee's, MMR® article on building a Stub Ended Terminal in an unused section of his N Scale model railroad. In Part 2, Jeff describes how he laid track and wired the track and signals.

On page 14, Steve Chapman MMR® shows how he fits lighting to buildings, then there is an interesting article from Dave Whibley on how he built a horse drawn wagon to complement his G scale distillery from page 16. Do you know what a transfer Caboose is? If not, turn to page 20 where Graeme Prideaux describes how to build one and what they were used for. T

There's more interesting and informative reading in this edition of MainLine, so keep turning the pages!

Meru Bagnall

Editor - MainLine On-Line

Building a Stub Ended Terminal

Part 2

by Jeff Lee, MMR®

Laying track, wiring, and signals

The mock up for the structures is progressing and some of the structures are being scratch built, but first we need to complete the track and wiring.

In the first part I shared the planning considerations, for my ethanol plant and some of the challenges I still had to face. I will describe these approaches below.

I am building a stub-end ethanol terminal on a 1800x500mm removable plywood base. Because it is removable, wiring and placement of the circuit boards like frog juicers, change significantly from a permanent layout. Most controls on a portable layout are attached to the underside of the base



board. On a permanent layout you can attach controls anywhere – within reason. I want to have the minimum connectors between the removable base and the permanent layout. However, there are basics such as the bus wires that need to connect to the main layout. Follow on and I will describe my journey and some of the challenges and lessons learnt.

On my permanent layout, I have most of the wiring and electronics at the front of the layout behind pull down fascia panels. This works well, but it is not what will work with a portable layout. With a portable module most of the wiring and electronics (*like FrogJuicers*) will be attached to the underside of the baseboard.

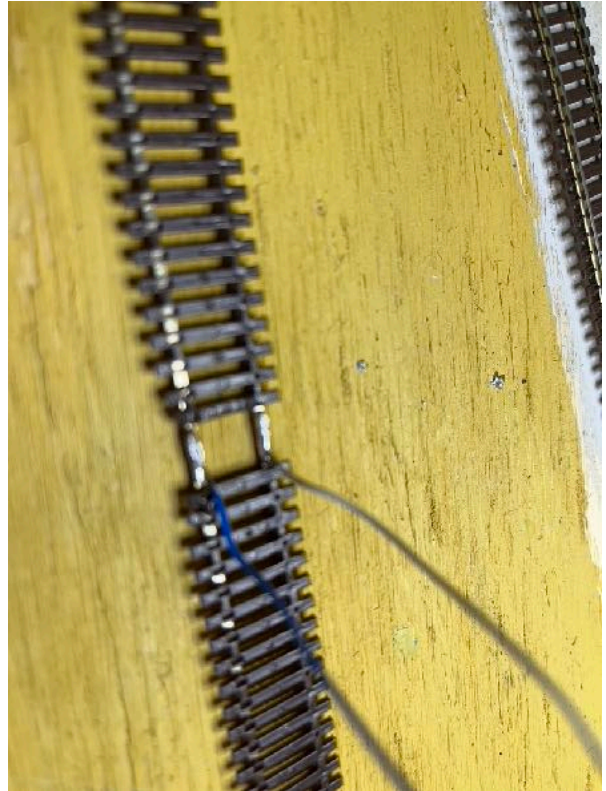
After successfully laying the cork roadbed, I am now going to lay track. As well as laying track I also need to consider where the bus wire will be best placed, how to power the frogs of the switches, and wiring for signalling. Logically the bus wire goes under the main lines. However that is also where the servos to control the switches will be positioned.

As you now know I planned to make this as portable / modular, as possible, so we don't want a large number of wiring plugs. The more controls I can attach to the underside of the shelf, provided they do not impinge on the frame, the better.

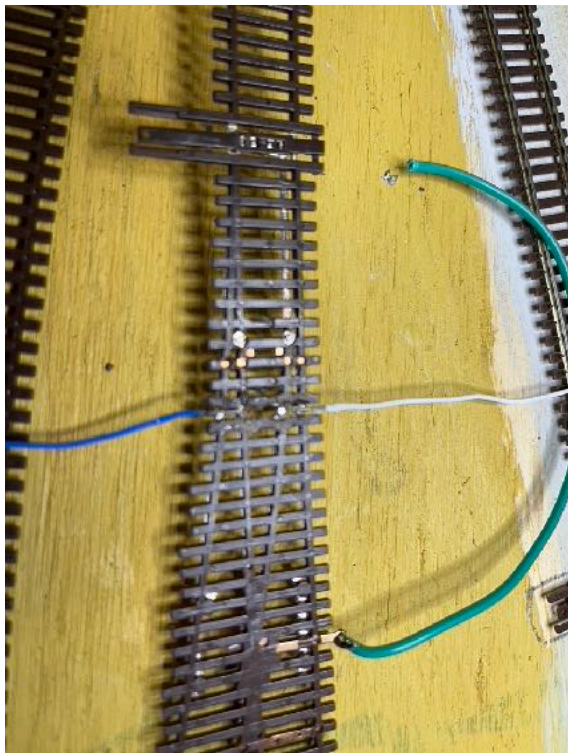
I also wanted to extend the 12V DC, 16V AC, and 5V DC bus wires to the module. To do this I used a 6 pin terminal connector and ran the six wires at the rear of the module base. The bus wires are stapled to the base board and terminated by screwing into a terminal strip at the end of the module.

Track Wiring and Feeders:

One of the lessons most of us have learnt is to wire every section of track to the DCC bus. I prefer to attach the wires under the rails of the track. The benefit is they are not visible when the track is laid. Some considerations are that you need to drill the holes for the feeders at the right location, but that can be fixed easily - see on. Another consideration is that in order to solder the wires you have to remove a couple of sleepers. The removed sleepers can be sanded down, so they slide in under the soldered wires when laying the track. Soldering the feeders involves using flux on the rail bottom and the feeder wire to connect to the rail bottom. Then both the wire and the rail is "tinned" with a little solder. If the soldering iron is hot then the solder should flow in a second or two and not harm the plastic ties. Once both parts are tinned bend the wire, place on to the rail and touch both with the soldering iron. It should not be necessary to add additional solder. If you do need more solder keep the



amount of solder small. Here is a section of track with feeders soldered.



As well as wiring the track each switch / turnout should ideally be wired. Relying on the track connectors to power the rails is not a good long-term solution as many of us have discovered. I follow a basic wiring standard. Blue wire is the back rail, white is the front and green is the frog lead. Below you can see the switch soldered with track feeders and the frog lead also soldered. Attaching the stock rails to the point rails ensures power is available to all rails of the switch and you are not dependent on the rail contact for power, even though Atlas switches have a connector already wired in., and this is still a good solution. Atlas switches also have the frog lead ready for attaching a wire.

Laying the Track:

Once the track and switch feeders are soldered, I connected up the tracks. Metal rail joiners are the most common, but insulated joiners are needed where switches face each other. There are variations on wiring depending on the brand of track so follow these guidelines as to where to put insulated rail joiners. Joining the tracks allowed me to see that all sections of track were aligned and also, where the feeders would go through the baseboard. At this stage I marked all the holes for the feeder leads with a "Sharpie" black pen. Then we drill those holes for the feeders. The next stage is to insert all the feeder wires and assemble the track prior to gluing down.

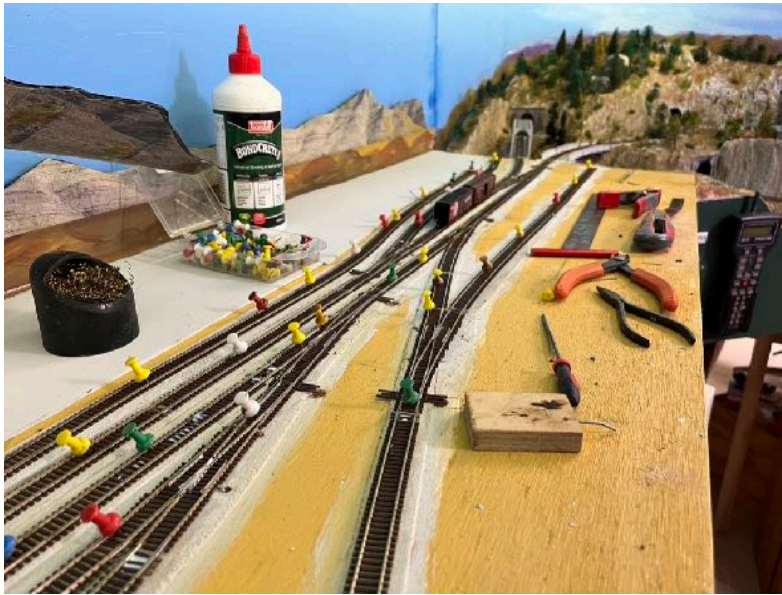
As I will be using servo motors to power the switches, I needed to drill a hole under the throw bar for the wire connection to the servo. At this stage I drilled a 10mm hole under the throw bar position. This is more than needed but once the servo is installed and working the hole can be minimised with thin styrene, cut to protect the throw of the servo wire.

At the connection between this portable section and the mainline I needed to secure the tracks robustly. It is key that the tracks at the connecting points are securely attached, so once the base board is in position the tracks are also aligned and will not move. To do this I used several circuit board sleepers soldered to the rails at both sides of the joint. Near the junction between the permanent layout and this module I removed some of the plastic sleepers on both sides and replaced with circuit board sleepers. These circuit board sleepers will hold the rails in gauge. I soldered the circuit board sleepers to the rails. Of course, you need to ensure the circuit board sleepers are isolated so there is no short. A quick file across both sides of the sleeper to remove the copper, will ensure there is no short. Bondcrete will attach the sleepers firmly to the roadbed. Once secured I will cut the rails at the gap with a fine razor saw., so we can remove the module

I start the gluing of the track at one end. Instead of PVA white glue I used Bondcrete which is a thicker similar product that is also waterproof when dry. I had drilled all the feeder holes and fed the wire feeders through the holes. For each section I raised the

tracks above the cork roadbed and if they did not support themselves on the feeder wires, I added some "chocks" to keep the tracks raised above the cork roadbed. Then I squeezed a film of Bondcrete over the cork roadbed. Using a paint brush, I spread the glue evenly and then pressed the tracks down. Once the track was aligned using a straight edge and eyesight, I used a combination of weights and pins to hold it down. Bondcrete dries within an hour





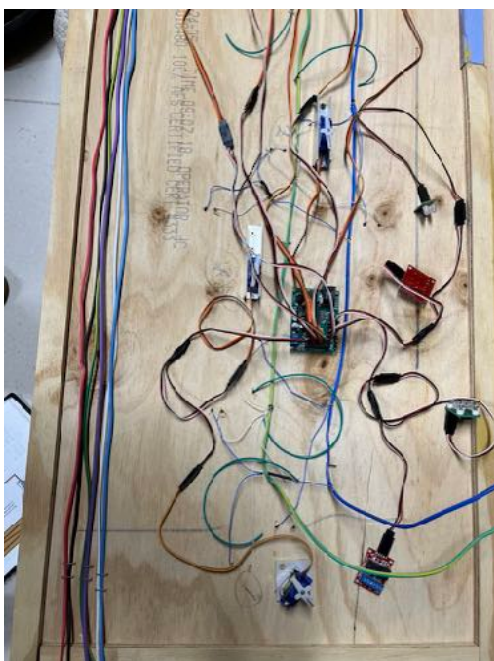
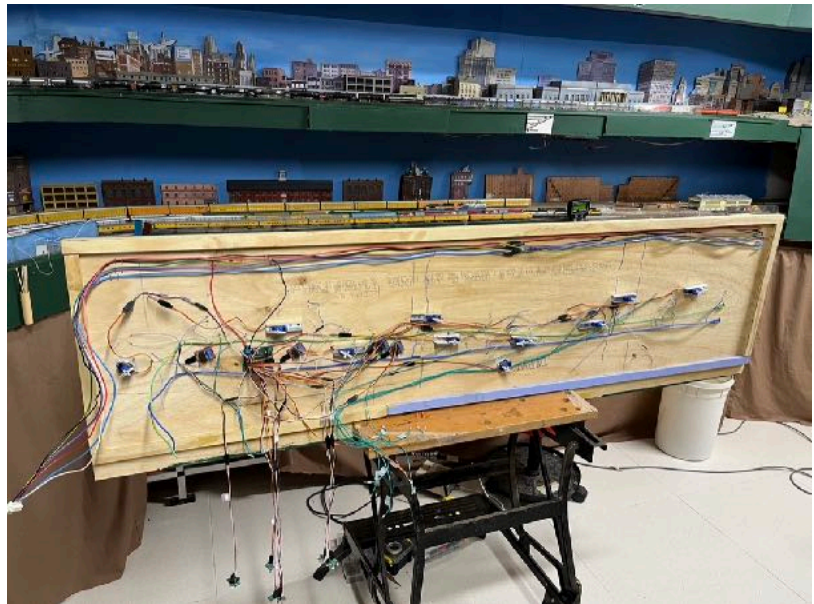
but reaches maximum strength in 24 hours, so I left it for 24 hours before progressing.

The next sections were similarly glued down, and left for 24 hours.

Once all the track was glued down I cut the two tracks leading into the terminal with a fine razor saw, and removed the module to wire the feeders.

One advantage of a portable layout is you can remove it and flip it over to solder the wires.

Here is the underside of the ethanol module. I attached the bus wires (*blue for back feeder and white for the front feeder*) to the baseboard using a staple gun. It is easy to solder the track feeder wires to the bus whilst sitting on a stool. It certainly beats



getting under the layout to solder or connect wires with Suitcase or T-Tap connectors.

I used a “workmate” to hold the module, and this made wiring much easier.

One lesson is to always label wires and controls. Each of the servos are numbered. Servo wires to the control panel are also labelled / numbered. It doesn't take a lot of time now but will save hours later.

The next step was to return the section and test. The bus wires connect to the Power District circuit breaker and Tam Valley reversing switch (*Dual FrogJuicer*) via lever wire clip connectors purchased in bulk from Amazon.

Connecting up the bus wires and turning on the power district we had our first test. Bingo, it worked! I have an on-off switch for every power district. This on-off switch is labelled and simple to isolate that section of track. This is a key feature which I strongly endorse. When a short occurs on the layout, and they sometimes do when a train runs a switch the wrong way etc, turning off every power district and turning them on one by one will identify where the short is.



However, even though we appeared OK, I discovered a couple of areas to fix.

1. Solder had spread from one of the connections to the track and flowed up the rail causing cars to jump. A quick fix was done with some filing. Once fixed the area was vacuumed to remove any possible metal filings that may find their way into motors.
2. In 3 locations the rail gap was more than ideal. I used 0.6x0.8mm Evergreen strip styrene. This was super glued into the gap and allowed to dry. Once dry using a sharp new blade the strip styrene was trimmed.

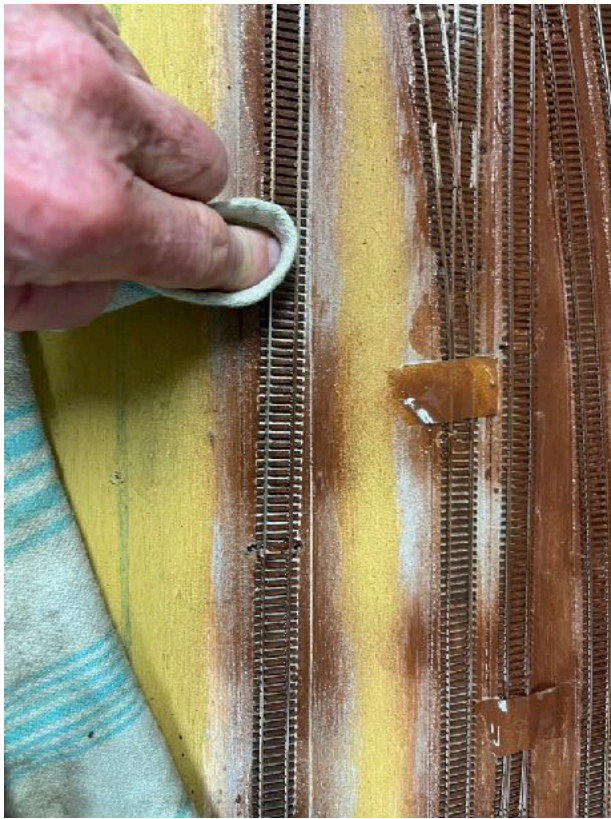
After that and some more track cleaning the new track worked well. This is a critical time. It is tempting to move forward but ensuring all the track is smooth at this stage is important as I have learnt many times. Fixing uneven track once scenery is completed is much harder than at this stage.

Track Painting:

Now the track seemed to be stable, it was time to paint the rails. The first stage was to use masking tape over the switch throw bars. This is key to ensure the contact between the throw bar and stock rail remains free of paint to ensure electrical connectivity.

I used Rustoleum Terrain spray paint in a "rattle can" to paint the tracks. The paint can dry





Above is me cleaning the tops of the rails:

To the right is the painted track.

Once the rails were cleaned again it was worth checking the tracks worked well again. Yes they did with a few small sections to clean.

Installing Servos:

The next stage is to install the servos to control the switches, set up a push button panel on the fascia, install the "Frogjuicers", and test.

I worked out which switches I needed to signal. I decided to use relays attached to the servos to set up the signals. Signalling would be simple. If the switch was closed to a line, the signal would be red. If the switch was open it would be green.

Another method of controlling the switches and signals is to use Double Pole Double Throw (DPDT) slide switches. This is cheaper than servos (or Tortoise switch machines). However, I decided to have all switches (10) controlled by servos with a push button panel on a track diagram.

If I was recommending track and controls, I would prioritise using Peco Code 55 and manual control. Peco switches have a spring that holds the switch when thrown. This would be cheaper than my method with Atlas Code 55 rail, and servos and controllers. On a "narrow" terminal like this all the switches are accessible for manual operation.

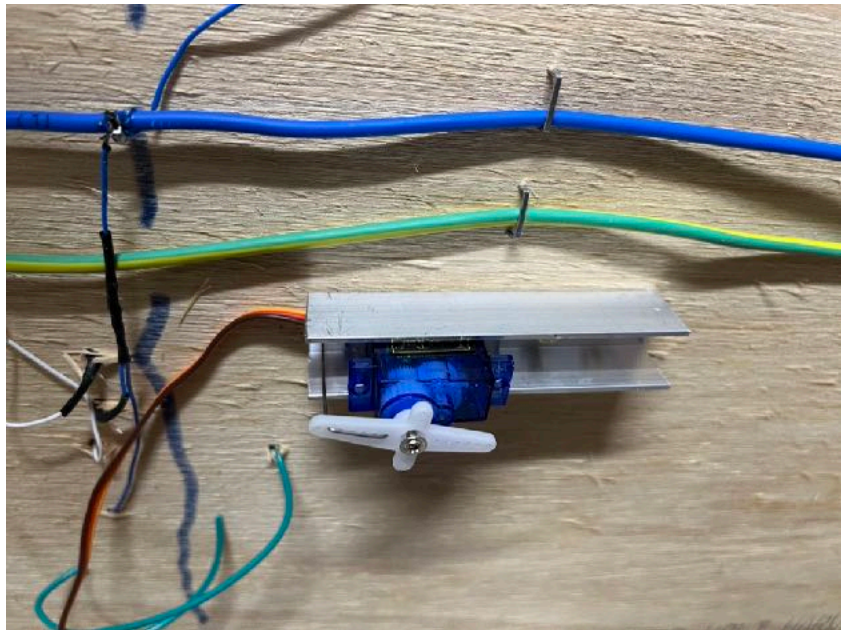
quickly so I did the layout in 3 sections. Once the first bit had been sprayed it was time to clean the tops of the rails.

I used an old rag, and where it became difficult to remove the paint, I added a little mineral turps to the rag.

Then we slowly progressed with more spraying and cleaning.



I used Tam Valley servo controllers. My servos are purchased via eBay. There are many ways to mount the servos. Several members have made 3D printed mounts. I used 16mm aluminium channel, as below, There is a small 2mm hole for the servo wire and two holes at the other end for screwing to the baseboard.



I ensure the servo is secure in the channel with silicone adhesive.

The servo will come with the lever centered, however it is preferably to double check this. Tam Valley offer a tool to do this, or you can use the servo controls on the board.

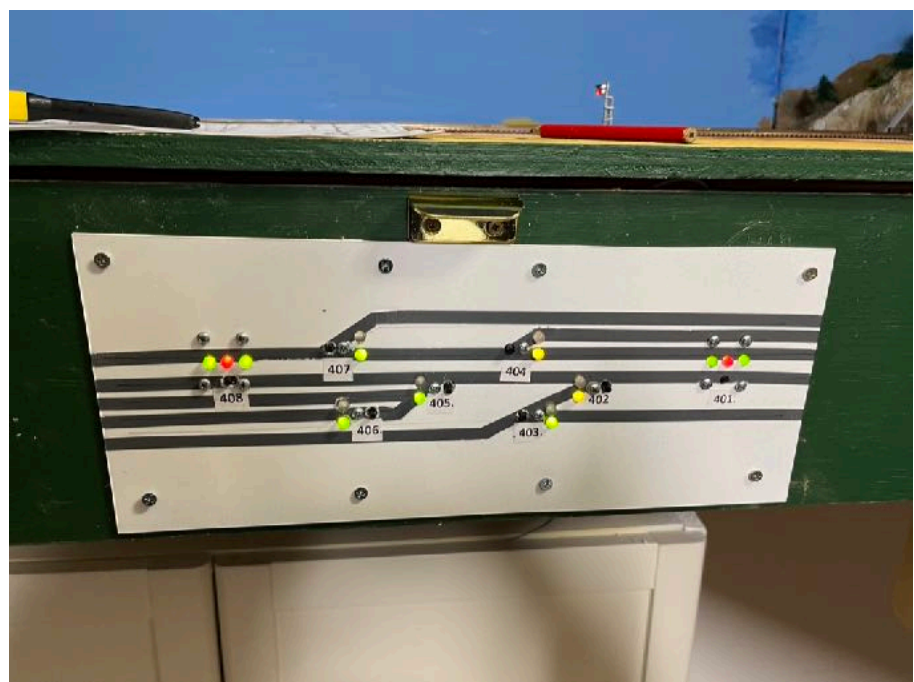
To attach the servo and channel I use double sided tape on the bottom of the channel. The throw rod is inserted into the throw bar of the switch through the hole previously drilled. Once centered, the servo is screwed to the base through the drill holes in the channel.

On the switches I decided to signal, I attached relays to the servos. My LED signals can take 12 volts but I found 5 volts gave sufficient light.

Adjusting servos can be time consuming but once set up they work fine with minimal maintenance.

Fascia Panel:

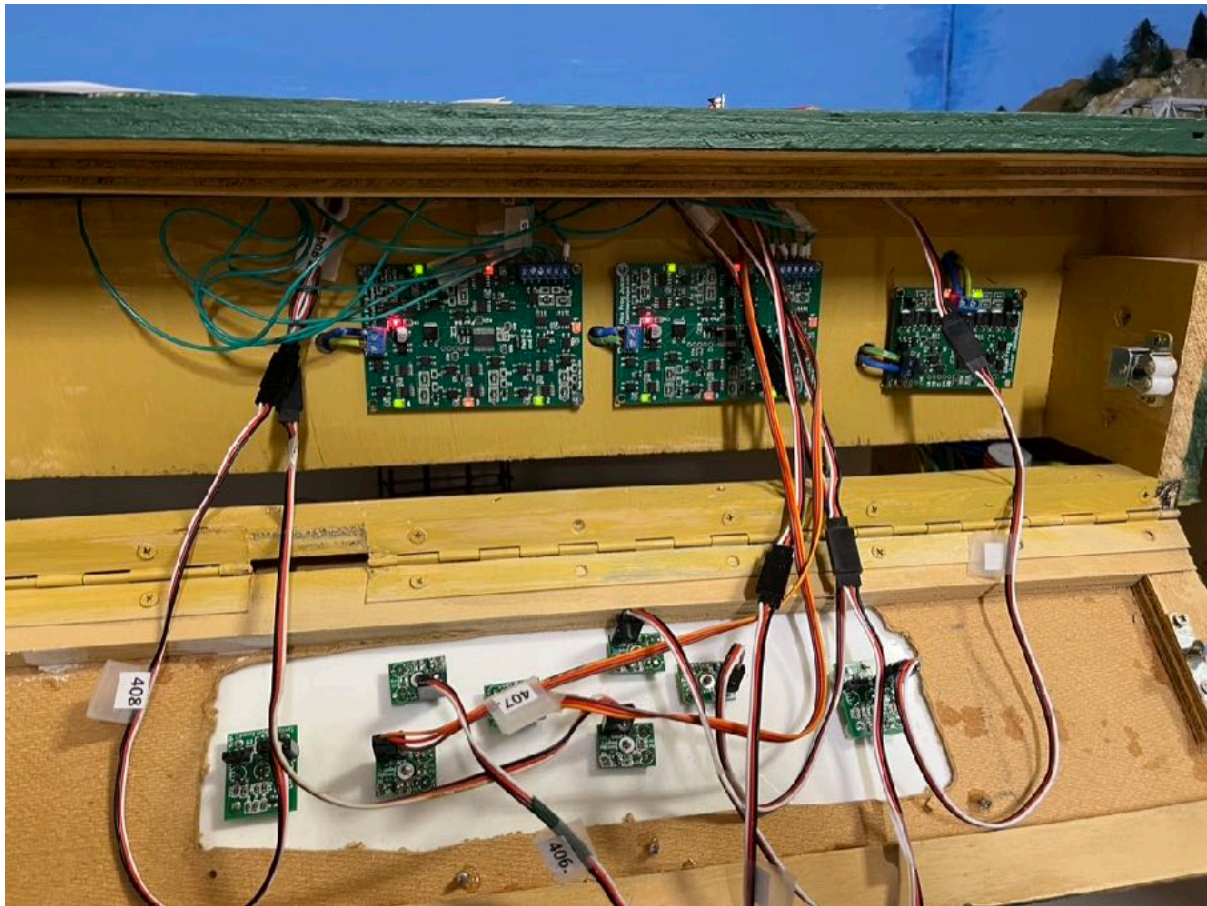
I used Tam Valley push buttons for the servos and switches. Each of the servo leads were labelled and numbered. Hey, do this, you will regret it later if you don't. My simple method is to print out the numbers and once cut out attach them to the wires with clear scotch tape. There are commercial labelling



products as well but this works and is cheap.

The fascia panel is 2mm thick styrene. This was spray painted black. I drew the track diagram and used 2mm masking tape to lay out the track plan. Then the panel was spray painted white. Once dry the masking tape was removed. Using a template for the push button controllers I drilled the holes for the push buttons.

The photo above shows the panel with the push buttons installed. Note that all the switches are numbered. This is power district "4". So, the switches go from 401 to 410, and the leads are similarly labelled. The numbers on the panel are printed on a laser printer and glued on with white glue.




The photo above is the back side of the panel showing the frog juicers and the reversing circuit.

A key lesson I would suggest is patience. Check the wiring and controls consistently. You will probably be OK but running a loco and checking every switch will ensure future reliability.

In the next episode I will describe what I find more enjoyable - building structures and scenery - but only if you get the track and controls working 100%.

Happy modelling.

You can see more of the layout on: www://jeffsrailroad.com

How to Fit Building Lighting

by Steve Chapman, MMR®

Lighting of model buildings can be lots of fun, and makes your scene look very realistic. There are many ways to light your model building, you could use those little lighting modules that sometimes come with your building kit.

You could use grain of wheat or rice incandescent globes.

The only problem with incandescent globes is that they get very hot due to the power passing across the filament causing lots of heat which in turn causes light.

This is an undesirable setup, as it causes plastic to warp and get hot. Which in turn could cause a fire which is very undesirable.

There are alternate lighting sources available, LED's are a very handy alternative.

LED strip can be purchased from online shops, or from most electronic shops.

LED's don't like to be short circuited and don't work at all if you have the wrong polarity.

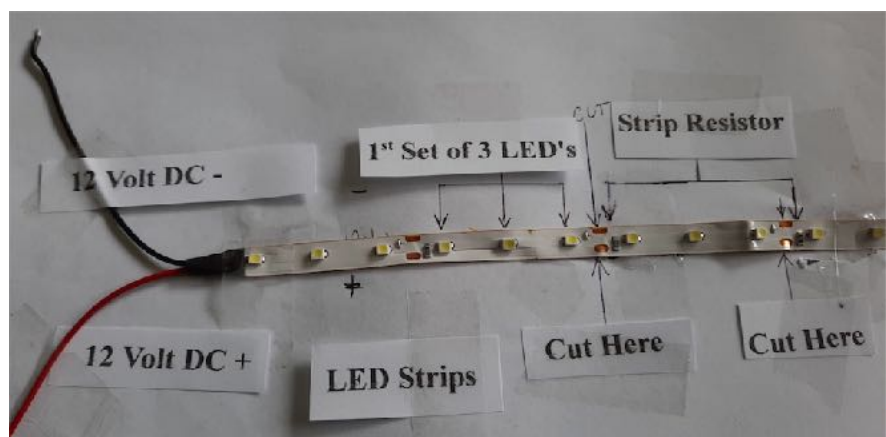
LED's will not work if you don't supply them with enough power.

LED's, if wired correctly and treated carefully should outlive you, so it's worthwhile to know how to use them.

The easiest LED's to install are the strip LED's, as shown to the right.

I brought my LED's on a spool just like the old 8mm film.

They come with a RED and Black wire for plus and minus and are rated at 12 Volts DC. You can also get them in 24 Volts as well.



You can also use Strip LEDs to light your passenger carriages, but you will need to add an Electrolytic Capacitor to keep alive in dead spots.

You will need to cut the LED Strips though the brass looking tab to separate them like in the low profile building back picture (see *photo below*).

I found that one set of three is more than enough light to illuminate the building.

Before placing the cut strip in the building fit plus and minus wires to one end and if extra strips are needed on the lower floor place a set of wires at the other end of the strip.

Fitting the wiring now is much easier when not attached to the building. Drill small holes in walls or roof so wires can pass through.

If you find the strips are too bright you can either place tape over one LED on the strip or paint it with black paint. Tape would be better in case you change your mind.



As you can see in the building back picture, I did not do what I said above. I found it hard to fit the wires this way. I found it's helpful to glue the wires to the wall with HOT glue. Just a little dab will do in case you change your mind and want to alter it. Hot glue will release fairly easily if you wish to reroute the



wiring. Be careful soldering on the RED or Black wiring to the brass tabs, not too hot otherwise you may damage the strip. A bit of Flux may help with the soldering.

After discovering the correct polarity try not to get the strips around the wrong way. Peel the sticky strip off the back of your LED strip and stick the LED strips to the ceiling of your building.

Try a dry run first just to make sure the polarity of each strip is correct. I did not want to redo the strips the way I mentioned in the picture as you lose the sticky back on the strips and need to stick on with hot glue. You try to learn from your mistakes and don't make them again.

All my buildings are not glued down, I have found they sit there with no problem. I have fitted a 3.5mm mono plug and socket to each lit building to make it easier to service the wiring or upgrade the lighting by adding an Arduino processor to get the lights to blink on and off at a set time.

I hope this article inspires you to light your buildings, it is lots of fun to do so.

Try putting curtains in your windows and furniture if you can. I used coloured paper with lines in the paper, and it works well. I use 1/48 dolls house furniture which is readily available in dolls house outlets. O gauge is OK but smaller gauges need to be more creative in finding furniture.

In the Picture of the building back, a junction box can be seen. This is used to terminate the wiring and also to hold a one only one thousand Ohm resistor (1K), which is used on the porch light which is a LED 3 volt street light. After removing the light from its street light base, I straightened it a bit and fitted it above the door to make a handy porch light. These fittings are also available on the net in the usual places.

Regards and best wishes for 2022, give lighting a try it's lots of fun setting them up....

A Weekend Project

by Dave Whibley

I have been thinking for some time about building a distillery for my G gauge layout. It will go at the side of a waterway with a water wheel as the power for the grinding of the grains.

My first thoughts were only for a basement and the floor above BUT in reality, that's not me, so the project got bigger and bigger until what I will have built in the end, will be 3 storeys high. My plan is to make each storey removable so that you can see the workings inside. MDF comes to mind as a material to use for the outside walls of the distillery, as I can carve that to make it look like stone and that will look good.

If you have a distillery, then you also must have a way to transport your products and for the era of my layout, a horse and cart would be the appropriate model to build. I have had a base with wagon wheels sitting on my G gauge layout for quite some time. Then the penny dropped. I could use this base to build my barrel wagon with. I purchased a 3D printer and it has been invaluable to make parts like arch bar boogies and wagon wheels etc.



In the basement of the distillery would be the storage area for the barrels of whisky and some wine casks and bottles. The next floor up will have bags of grain and grinders to make mash which will be stored in the brewing vats.

Also at this level will be the motors for pumping the mash up to the kettles for brewing. This activity requires lots of pipe work. As I want to be able to have each level removable, I will have to put in a fair amount of thought in to the construction for this to

be achieved and all line up perfectly each time I take the model apart for showing.



The photos show you how the wagon has been put together. I use a nail gun when putting my models together and I sometimes wonder if I am the only modeller who uses that technique to put models together?

The original base (which had been sitting around) was a fixed wheel

construction. But to make a wagon which is horse drawn, I needed to change the fixed wheels to swivel. So I cut the smaller wheels from the base and turned them around, and pinned them so that they would swivel. Every good wagon needs a braking system and this wagon has a hand lever brake. Now, on to the base went the sides to make the wagon. The sides were made from old redwood. The top plank is splayed out and this has been reinforced with steel wire. A trip to the toy store was inevitable and there I bought two Clydesdale horses. One looks like the father and the other is his ditzy son just there for the muscle power required to pull the wagon. Then I needed to research how horse rigging was put together, so thankfully Google is available. I made the back straps out of gasket paper and the reins that control the horses is brickies string-line. The modern string-line is now painted bright fluoro colours, and what I had was bright pink but brown paint covers everything and also stiffens the rope up too. These reins are fitted to the bit and yoke using small nails drilled into the body of the horse.

My friend Frank Godde made the bloke on the seat. He is made from casting plaster from a rubber mould. I altered the position of his arms and gave him the whip which is wire with cotton wrapped around it. But then he needed a wide brimmed hat as well which I made out of cardboard.

The 20 barrels on this wagon are made using chipboard, wood filler, and gasket paper



(for the straps). Starting with a 25mm hole saw, lots of disks are cut out of 12mm chipboard as I need four to a barrel. Then 4 disks are glued together to make the correct height for the barrels on for my G gauge wagon. Then 5mm square redwood is glued around the edge of the disks. As this is circular, the backs of the 5mm square redwood are touching leaving a slight gap at the front. These gaps are then filled with wood filler. Once

this is all dried, I put a spindle through the chipboard holes, and bolt it tight. The spindle is then put into the drill, drill is turned on and the barrel is shaped using the belt sander. The hole saw is then used without a drill bit in it to cut 3mm veneered disks to make the lids and bases. These will have to be scribed to show the wood grain and planking.

Gasket paper is then cut to size to make the barrel rings. The rings for the top and bottom are equal in circumference and the middle ones need to be slightly bigger. The gasket paper is then painted black and glued to the barrels. The last thing to go in, is of course, the bung.

For this wagon, this process was repeated 20 times and then all the barrels are tied down to the wagon with hemp string.

In real life when these barrels are empty, they would weigh approximately 80kgs each, but when full of whisky each

barrel would weigh a lot more. It is obvious to all of us, that this wagon is therefore on the return trip back to the distillery having unloaded at the hotel, and the driver has picked up the extra barrels to be returned and filled again at the distillery.



I have really enjoyed making my weekender project.M

Magazine Publishing Deadline Dates

If any member wishes to submit **An Article** for publication in MainLine, your article may be submitted at any time and it will be included in a future edition, where the subject matter will allow for a balanced number of differing subjects to be included, and where the number of available articles will allow for that to occur.

If you are providing any type of report, then **All Report Types** can be submitted at any time with a deadline date being as shown below, which is 10 days prior to the end of the month of publication.

This criteria is requested to ensure that the editor has sufficient time to complete the bi-monthly edition of MainLine in the required time frame.

If you are providing a **Divisional Meeting Report**, please submit your report as soon as possible after each monthly meeting, with the deadline date being as shown in All Report Types below. If your meeting is scheduled after the deadline date, then the cut off date is three days prior to the end of the month as shown.

This criteria is requested to ensure that the editor has sufficient time to complete the bi-monthly edition of MainLine in the required time frame.

File Types:- For all submissions, text files saved as MSWord, Pages or Open Office files are preferred with limited text and page formatting. Please don't send pdf files, they are unsuitable for use in this publication.

Photo Types:- For all submissions, photographs are preferred as jpegs or png file types and to be under 2MB in size. Should you prefer to send larger photo files, then please consider sending them via Dropbox or Google drive or a similar 'Cloud' storage program, or alternatively send a disk or flash drive via a postal service.

The following are the deadline dates for the next two editions of MainLine:-

January / February 2023

Deadline date for All Report Types = 21st December, 2022

Date for Reports of Div Meetings that occur after the Deadline date = 28th December, 2022

Publish Date on Web = < 5th January, 2023

March / April 2023

Deadline date for All Report Types = 18th February, 2023

Date for Reports of Div Meetings that occur after the Deadline date = 25th February, 2023

Publish Date on Web = < 5th March, 2023

NMRA-AR Division 7 Mini-Convention

Your Invitation!



November 12th-13th 2022

Venue

Bowen Mountain is a small community in the City of Hawkesbury located in the foothills of the Blue Mountains. **Bowen Mountain Park**, the venue for our Mini-Convention, overlooks the towns of Richmond and Windsor and is central to the surrounding Bowen Mountain area. The Park has stands of native and exotic trees and offers panoramic views across the Cumberland Plain and the Sydney Basin.

Getting There

Getting to the Mini-Convention is best via Kurrajong, the nearby commercial centre, which is where the Mini-Convention Dinner will be held. After driving carefully up to Bowen Mountain Park, car parking is available in the carpark on Lieutenant Bowen Road. There is limited alternative parking for attendees with mobility needs from Bunya Cres.



Audience

All NMRA-AR Division 7 members are invited. There is a limit of 40 attendees for this Mini-Convention. Invitations will be offered to other Region Divisions including ACT and upper NSW. Seats available of a 'first in' basis.

Costs & Payments *

Mini-Conference Fee: \$40.00 (\$70.00 for non-NMRA**)

- Attendance at all Clinics, including morning and afternoon teas, and lunch on Saturday and Sunday.

Saturday Evening Mini Convention Dinner: \$60.00

- Three course dinner at The Village Kitchen Kurrajong located in nearby Kurrajong. Drinks at own cost.

Payments:

Pre-Paid during your online registration:

- Mini-Convention Fee, Lunches *plus* Saturday Evening Mini-Convention Dinner.

Paid Direct to Accommodation Establishment:

- Accommodation and other meals not mentioned above

Registration

Via this link => [Register Me for the Mini-Convention](#)

Registrations close at 40 attendees or on 31st October 2022, whichever occurs first.

* A Booking Fee and Transaction Fee are applied during the payment process

** Includes 1 year NMRA Membership

Timetable Overview

Doors open at 9.00am both days for tea/coffee and mingling. Welcome at 9.25am and morning tea is at 10.30am both days. Lunch at 12.00pm on Saturday and 12.30pm on Sunday. Afternoon tea/coffee is on at 3.00pm on Saturday and 2.30pm on Sunday. Saturday concludes at 5.30pm and 3.30pm on Sunday with a farewell from Division Superintendent John.

Layout Tour Opportunity

Division Superintendent John's Layout is available for a brief visit on Saturday afternoon at 2.00pm and again on Sunday morning at 10.30am.

Numbers are limited to 10 people per visit, but most definitely worth the short car ride to view!

Model Contest Judging Categories

Three categories are available for you to submit an entry:

- 1) Kit Bashed
- 2) Scratch Built
- 3) Electronic Technology Effects/Actions

Meals and Refreshments

Morning tea, lunch and afternoon tea included on both days. However you may BYO lunch if you so wish. No alcohol is permitted at the hall.

Dinner is at [The Village Kitchen Kurrajong](#) (licensed premises) in Kurrajong on a pre-booked and pre-paid for basis. Two entrées, three mains (gluten free) and two desserts to choose from and selected as you complete your online registration.



Please inform us of any dietary requirements as your complete online registration.

Saturday Night Accommodation Options

Accommodation options are available in nearby Richmond. Five options are suggested and you will need to make your own bookings direct with the establishment of your choice.

- [Colonial Motel Richmond](#)
- [Crown Plaza Hawkesbury](#)
- [Hawkesbury Race Club Motel](#)
- [The Richmond Inn](#)
- [Archibald Hotel](#) - at Kurrajong Heights

N.B. Check dates, room type, number staying, etc

Note: all images and text links are 'active' for your convenience



Clinics

Saturday

- ⇒ **Inexpensive Accessory Decoders and Servo Drivers**
with Erik Bennett
- ⇒ **Riverbelt Line Shelf Layout**
with Paul Cassar
- ⇒ **NMRA Achievement Program**
with David O'Hearn MMR
- ⇒ **Photographing Models—the legless ones!**
with Gerry Hopkins MMR

Sunday

- ⇒ **Prototype-Inspired Layout Design, Construction and Operations**
with Gary Glazebrook
- ⇒ **Battery Powered Remote Control**
with Brian Anderson
- ⇒ **JMRI An Overview from a User's Point of View**
with Les Fowler

For more information:- <https://www.facebook.com/groups/nmraardiv7>

CR&P Transfer Caboose

By Graeme Prideaux

The photo depicts CRI&P (Rock Island) Fairbanks Morse H15-44 road switcher 400 with a transfer caboose passing the gas storage tanks at Yallambee Road on the Wayne sub division of the Queensland NMRA modular layout during an operating session in June 2021.

While the road switcher is an Atlas product, the Rock Island transfer caboose was scratch built by long standing member Ian Venables. The chassis is from a 1950s Central Valley flat car and bits and pieces of spare material that strategic modellers always keep in the 'junk box' in case there is a need in the future. The trucks are Central Valley Bettendorf type of the early 1960s and the couplers are contemporary Kadees. A little surgery was needed to the chassis in order to mount the couplers in accord with standards.



Ian believes the part number for the chassis was "CV1". He also said that flat cars are easy to build from this part number or from scrap pieces of Evergreen styrene kept on the workbench. A lot of

spare parts or complete requirements for fitting out a project are utilised by Ian through using the surplus material and fittings that are stored on the workbench.

Transfer cabooses were specialised vehicles that railroads used in the caboose era to move cars between their own and the yards of other railroads in a local area. Generally, transfer cabooses did not contain all the facilities that were provided in cabooses that were used in long haul traffic.M



[ED;- Shortly after I advised that I would include this article in the last edition, sadly the builder of the Transfer Caboose, Ian Venables, has passed away. Ian had been an NMRA member for 35.2 years, he was an amazing individual and an outstanding modeller. Ian's passion was scratch building rolling stock, especially freight cars, and he built over 250 of them. He was a true master of his craft, none were freelance, they were all the product of extensive research, there had to be a prototype, often they were connected to his favourite prototype, the Louisville and Nashville. Ian was the editor of the Division 1 newsletter up to April 2018 and was a fine modeller. May Ian now rest in peace and in no more pain and suffering. There is more to be said about Ian in the Division 1 August report, which is located further on in this edition.]

100% NMRA Inc.-AR Club News

Adelaide Model Railroaders Inc.

A 100% NMRA Club in Division 6

Club News - Running Night **September** 2022

By Ken House (AMR Newsletter Editor)

<https://adelaidemodelrailroaders.com>

We had our best roll up for an operating session so far, eleven people, including two visitors (prospective members?), rolled up for the **September** Operating Session. I would like to remind club members that while we welcome visitors, especially those who may become future club members, our insurance allows visitors and prospective members to come along for three months only for prospective members and three times for visitors. Then they must join the AMR.

Crews for the evening were, Warwick Graham - Kingston yard master, Tony Mikolaj - Houseman yard master, two man crews, Peter Kirkland and visitor Mark, Murray Dent and visitor Jarrod, Paul Atkins and Rainier Davenport, then single man crews Paul Wright and John Prattis. Finally train master/dispatcher Ken House.

Right: Left to right at Houseman yard Tony Mikolaj the yard master, Paul Atkins and Rainier Davenport who are crewing the Houseman turn which has arrived in Houseman from Kingston. Their GP 7 is in the foreground.



Since the sequence had been completed last month, the layout had been re-set and re-staged

back to the beginning of the sequence. The morning sweeper, the first train in the sequence departed Barclay staging at about 7.40pm. Because some trains struggled on the Southern Central's grades on previous sessions, maximum train length for trains on the graded parts of the layout were restricted to a length of twelve cars and caboose. No trains struggled with the grades this session and most trains ran quite well. However some did not run all that well. It appears that the track was still somewhat dirty in places and wheels could have done with some cleaning. During the session we cleaned the wheels of the two Southern Central Baldwin switchers as well as GP7 923 on the Houseman turn. More train running between sessions will help to achieve smoother running, the more you run a layout the better it will run.

I did notice that not all operators can identify the various types of freight cars used on the SCRR. Because we are not using the reporting marks on the freight cars to identify



freight cars, it is important that operators can tell the difference between a box car and a reefer, and a three bay hopper from a two bay hopper, so that the industries along the SCRR get the right commodities delivered.

Above: The morning sweeper runs past Matthews Machine company limited as it arrives at Houseman where it will set out a cut of six cars which will eventually go into some of the industries at Houseman or onto other towns along the SCRR in either the Kingston turn or Houseman turn.

As the session was drawing to an end, three trains were converging on Zieglersdorf. The Houseman turn east was rounding up its train having just completed its switching moves at Zieglersdorf, while the empty cattle train west was approaching from Letheby to set out empty cattle cars at the Zieglersdorf cattle pens. At the same time the Kingston turn east was at Atkins about to approach Zieglersdorf from the other direction. When the session did actually end the Houseman turn east had completed it's work at Zieglersdorf and was able to proceed to Letheby and the empty cattle train west arrived at Zieglersdorf, ready to set out the two cattle cars and the Kingston turn east was still in the loop at Atkins.

So this session ended with three trains sitting in consecutive passing loops, Atkins, Zieglersdorf and Letheby, two east bounds and a westbound.

Right: GP7 number 930 passing below Haynes bridge with the Petrol train. east. The Petrol train came onto the SCRR at Barclay with four tank cars of petrol (gas or gasoline in America), it picked up another ten tank cars at Opie oil refinery. Although an east bound on the SCRR it



will head off on Santa Fe rails after leaving the SCRR at Jeremy Junction on Cooke's cut off . Once it is hidden in the tunnel on Cooke's cut off it is deemed to be away on Santa Fe rails. Cooke's cut off tunnel number one being considered hidden staging.



Left: GP7 number 923 switching Kerry canning at Werkendam while working the Houseman turn west. The Houseman turn begins at Kingston as a west bound travelling to Houseman where after setting out it's train and turning the loco it picks up a new train before departing

east and proceeding to Kingston with switching along the way.

Right: Kingston switcher, SCRR Baldwin number 122, with Jefferson abattoir in the back ground is setting out empty cattle cars on the yard track. Because this track has a 1.5% grade a plastic "blue flag" pushed into the track behind the last car prevents the cars from rolling away. The "blue flag" means danger and can only, according to AAR rules, be removed by the operator putting it there.



Above: Looking in either direction at Kingston we have operators, left, Paul Wright ,and right, John Prattis.

After the session ended all retired to the meeting area for tea, coffee and cookies for supper. All operators said they they had enjoyed the session and are looking forward to the next session.

WORKING ON THE SOUTHERN CENTRAL RR

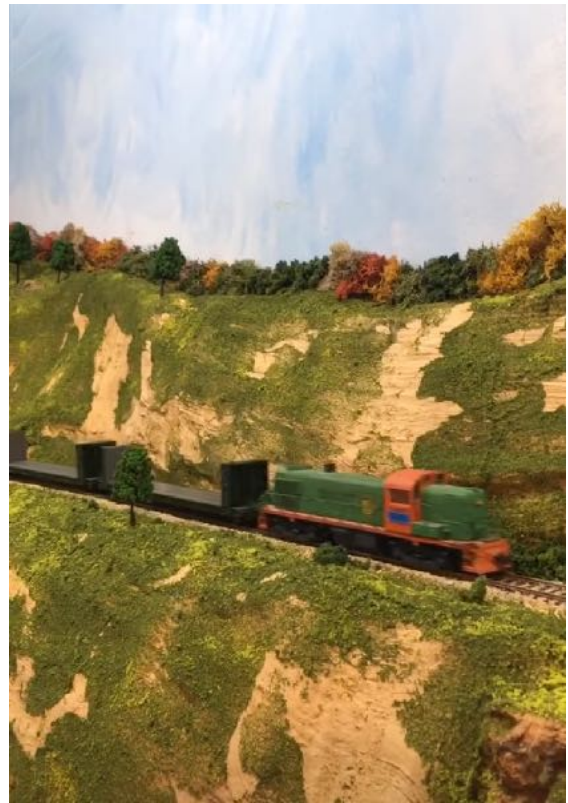
Having been ordered from Walthers on August 24 2019, the twelve bulkhead flat cars finally arrived on September 7 after production problems and supply chain delays due to Covid. These cars will carry RSJs (rolled steel joists) and similar steel shapes from the RHP Steel rolling mill. They are slightly later than our era, but close enough for our purposes.

Right: New bulkhead flats behind a RS3.

Short video below:

<https://www.youtube.com/shorts/A4vBEslh46M>

Below: The new tunnel Tony Mikolaj is installing at the end of the peninsula east of Wallage Wells. The road for to the level crossing can be seen in the right photo. The tunnel covering is removable. {Photos Tony Mikolaj}



Paul Wright has completed Walthers kit Interstate fuel and oil. This bulk fuel dealer is the final industry at Kingston.

Adding the bulk fuel dealer to Kingston does not mean that work is finished in that area of the SCRR. The area now requires detailing to bring it to life. The structures should be weathered lightly for most heavier for the quarry structures. Roads, trees, fences, drums, boxes, crates, pallets, road vehicles, cattle and people should be added thoughtfully. For instance I believe that people should be posed in stances of little movement eg sitting, standing waiting for a train, standing in conversation, or about to lift something. People jogging, running, jumping, or marching do not quite work in model railway scenes, too much movement implied. Also Kingston needs a caboose track.



Left: Mikolaj fuel and oil is the new industry at Kingston. Built by Paul Wright it is a Walthers Interstate fuel and oil kit. Mikolaj fuel will receive petrol and diesel in tank cars as well as oil, grease etc in drums in box cars.

Above Right: click on image above for You-tube video of operating session[M](#)

Turn to page 72 for the October update from the AMR.

100% NMRA Inc.-AR Club News Wide Bay Burnett Model Railway Club Inc.

A 100% NMRA Club in Division 1

By Stephen Reeves - Club President

Club and Layout Construction Update **September 2022**

Prior to the latest working bee, originally planned for Saturday 10th September, but postponed to occur prior to the monthly meeting on Saturday 24th September, I prepared "leg mounting" or attachment blocks and cut the aluminium square tubing for the leg sets to ensure we could maximise the use of our time during the working bee.

These blocks provide the connection point for the module supporting legs, (*to be fabricated using square hollow aluminium tubing*), to attach to the modules via hinges, which will allow them to fold underneath for transport.

I measured and cut the blocks from timber stock I had at home. I started by ripping all the pieces to the same width using my Triton saw bench. I then took measurements from each end of the three layout modules, taking note of the location of the steel



Leg Attachment Block showing notching to fit around steel support

supports and their attached tabs which were detailed in the previous article, so that I could notch

out the timber to fit around the steel supports.

Leg attachment block showing hole for connecting bolt access

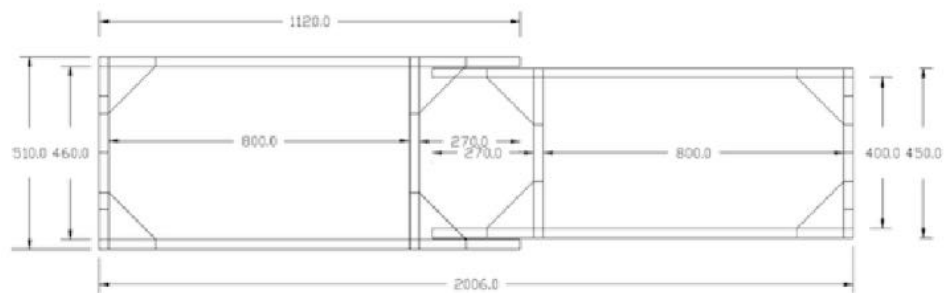


Plain leg attachment block for outside end modules. I attached the hinges to all blocks in preparation for attaching the legs.

Furthermore to save time I completed an AutoCAD drawing to work out exact dimensions for the supporting legs taking in to consideration the final viewing display height of the modules and the length of the modules, which would determine how the legs would fold up underneath for transport.

Whilst carrying out the drawing I concluded that the ends of the legs would overlap when folded and therefore have arranged for one set to be wider than the other set.

In addition I had to work with the pre-cut notches in the bottom of the module cross-section supports. These dimensions restricted the maximum and minimum widths of the sets of legs.



These dimensions restricted the maximum and minimum widths of the sets of legs.

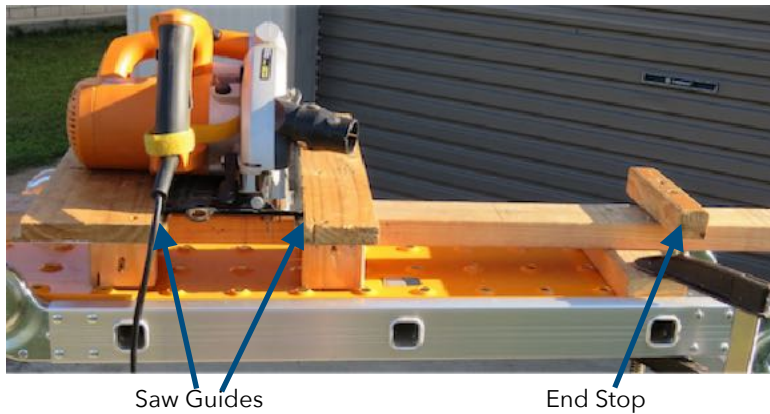
I have been using CAD for over 20 years, and it is fantastic for producing an extremely accurate drawing with minimum time and effort. Drawings can be reworked very easily, plus they can be stored on your PC or Hard Drive and may be printed out whenever you require them, as well as viewing or printing them at any scale or magnification. I am most proficient in drawing in 2D and the free program I have doesn't have all the features to be able to draw in 3D.

The legs will have tube inserts, with M10 internal threads, fitted to them, to allow levelling feet to be attached. I have also included or planned for the fitting of gusset plates to further increase the rigidity of the aluminium leg sets.

We had already purchased the Qubelok® 25.4mm x 25.4mm (1"x1") aluminium tubing for the construction of the legs.



Approximately 18m of aluminium cut to precise lengths for fabrication of leg sets



I cut the aluminium on my "home-made" aluminium cutting jig.

As can be observed from this "operator view" I assembled my cutting jig from timber and designed it to take a 185mm (7¼") circular saw fitted with an aluminium cutting blade. I recessed two self-drilling square drive pocket screws into

a timber batten to allow me to precisely fix the end stop so I could repeatedly cut multiple pieces to the same length easily without having to measure and mark each piece of stock. I clamp the jig on top of the work platforms on my multi-position folding ladder, which provides a safe and stable stand at a very suitable working height.

As shown in the photo, I fitted a screw to fix the blade guard in a retracted position to allow the saw to move freely in its' guides. This modification has also proved its worth when using my saw in my Triton work bench.



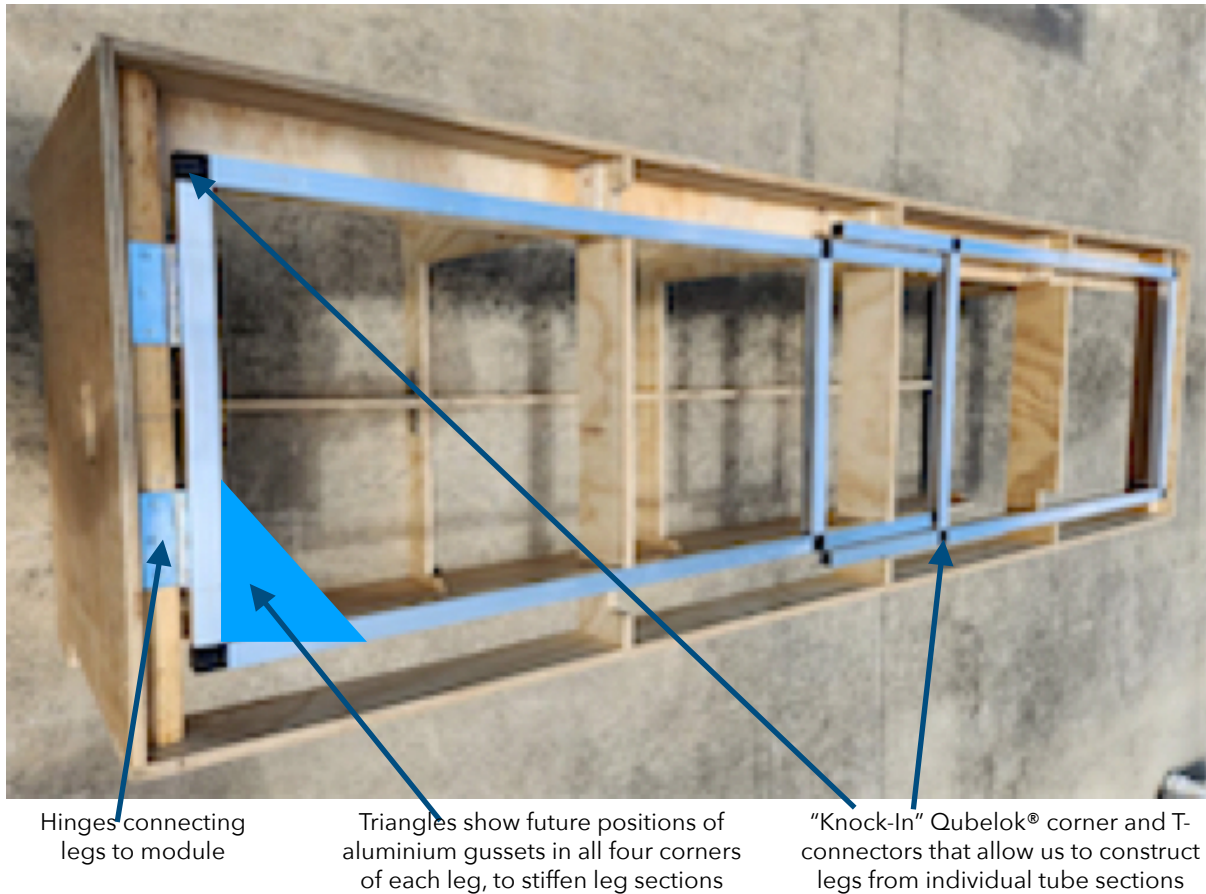
Aluminium stock clamped against timber support for safety and to ensure piece is cut squarely

Aluminium cutting

Retaining Screw to hold blade guard in




On Saturday 24th September, as planned, we met for our working bee and Kerry and Cory assisted me to screw the leg attachment blocks to two of the modules. I also made up four leg sets by knocking the aluminium sections together with the plastic Qubelok® connectors. We were able to screw legs to two of the modules.



The photo above shows the end module with legs attached in folded position for storage or transport. The module is upside down in this photo.

This is the centre module standing on its legs. Once we install struts or other forms of bracing the modules should be quite stable in their standing position.



Next time we hope to attach the legs to the last module, fit the threaded inserts and levelling feet and possibly fix packing pieces on the inside of the modules to provide "end stops" to prevent legs opening past right angles. Future work anticipated includes priming the modules to preserve the plywood and protect against weathering, possible damage caused when handling the modules, and to stop water penetration during detailing of the layout, for example, when gluing down ballast. Also, we anticipate that we will have to complete design of the fiddle yard and connecting sections in preparation for getting these pieces cut out by CNC, prior to assembly.....

100% NMRA Inc.-AR Club News

Wide Bay Burnett Model Railway Club Inc.

A 100% NMRA Club in Division 1

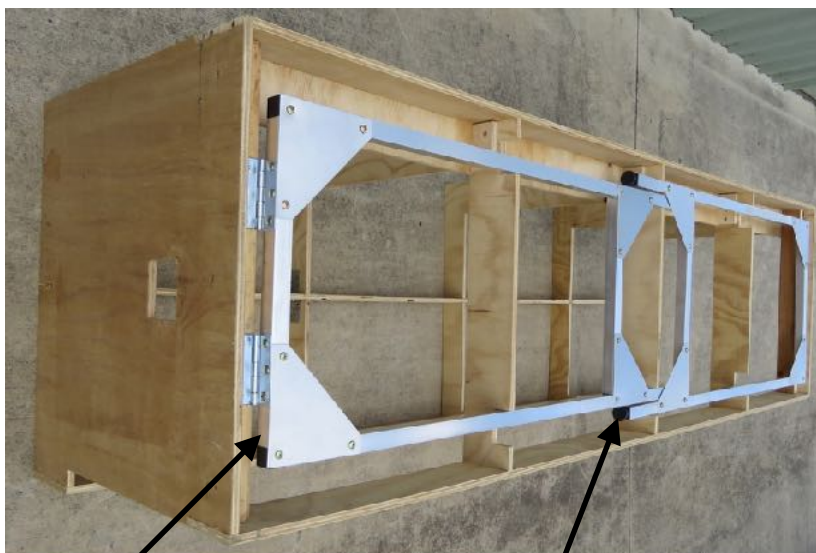
By Stephen Reeves - Club President

Club and Layout Construction Update **October 2022**

Kerry Bucholz and I met together on Saturday 15th October for a shorter, though very productive layout construction working bee.


Previous to the working bee, as mentioned in the last update, I obtained the gussets from a local business, and received the inserts and levelling feet from a company based in Perth. Due to the room taken up by the levelling feet I had to cut 20mm off the bottom pieces of the leg sets so they would fit when folded up.

Kerry constructed the leg sets for the last module, and fitted all the leg inserts and screwed in the levelling feet. I screwed the gussets to the leg sets of each module.



Corner Reinforcing Gussets

Levelling Feet

As mentioned in the September update future work anticipated includes priming the modules to preserve the plywood and protect against humidity, painting the steel end brackets to prevent corrosion, and fitting locking mechanism or braces to lock legs in standing position.....



Gussets and levelling feet detail

NMRA Inc.-AR

Achievement Program Report

by David O'Hearn, MMR®, AP Manager

Web Site Changes – As you are aware, the Australasian Region has changed their web site which makes it much more difficult to find all the Achievement Program information. To find the AP information, on the home page select the “Members Only” pull down menu. Then select “Education”. Then select “Achievement Program (AP)”. This opens the AP top level page. Then click on the words “AP Requirements” highlighted in bold red type. This then sends you to the US web site AP Requirements Section. You then move up one row in the right hand titles box and select “Categories” to find the requirements and documentation for each AP category.

I have made comments to the AR President and others to at least get a link to the Achievement Program on the AR web site Home Page so you can quickly locate AP information.

Model Railroad Engineer-Civil

In this article I will cover the requirements for the Civil AP. The AP has three requirements sections that I will address in turn.

Part 1 – Scale Track Plan

This requirement is to draw a track plan of your layout showing the various features stipulated in the AP Requirement (items A to D). Note that the scale of the plan is not stipulated so the plan can be drawn to fit an A4 sheet of paper. I suggest drawing the track plan, then making copies before labelling the track features. That way, the same track plan can be used for your Electrical AP.

Part 2- Construct Track work

This section requires you to build a minimum amount of trackage (depending on the gauge you are modelling) and to include six of the listed features in the SOQ. The track should also be “detailed” with ballast, drainage (where applicable) and I would suggest painting of the rail and sleepers. All of the items and trackage in this section can be commercially purchased items. If you are building a home layout or an exhibition layout, you will probably be doing all this activity anyway.

Part 3 - Scratch built Track work

This section requires you to scratch build three track items from the list in the SOQ and for the items to achieve a Merit Evaluation. If you look at the Evaluation form, the track work must meet a “go/no go or pass/fail” for each of the following requirements:

- Workmanship: A self-powered locomotive successfully traverses all routes.
- Prototype Practice. All applicable NMRA Track standards (S-3) are met using an appropriate track gauge.

- Scratch built frog and points. Commercial frogs not allowed but commercial individual rail (not Flex-track), ties and spikes are allowed.

You do not have to achieve a set score for each of these requirements. Tools such as a Fasttracks jig can be used to make the track and to manufacture point frogs, etc. You do have to wire the trackwork to allow the self-propelled locomotive to traverse the routes.


The scratch built track work does not have to be on your layout. It could be constructed on a sheet of scrap plywood. For my AP, I made a turnout, a crossover (which is really two turnouts) and a 90-degree crossing. As I model NSW railways, they did not use 90-degree crossings so I built this crossing on plywood and consigned it to a dumpster after I was awarded the AP.

Conclusion

Although an initial reading of the Civil AP Requirements may seem daunting, if you are building a home or exhibition layout, you will be doing Parts 1 and 2 of the SOQ anyway. Part 3 stretches you to build some trackwork but once you have mastered this skill, you will find your turnouts will work better than the commercially purchased ones and you will have more reliable running of your trains.

Recent Awards

I would like to congratulate the following members who have attained awards in the Achievement Program since the last issue of MainLine:

Golden Spike -	Nil this period	
Association - Official -	<u>Division 6:</u>	Jane Robinson, Flagstaff Hill, SA Ron Solly, Evanston Gardens, SA
Association - Volunteer -	<u>Division 1:</u>	Merv Bagnall, Hervey Bay, QLD
	<u>Division 7:</u>	Les Fowler, Seven Hills, NSW
Model Railroad Author -	Nil this period	
Chief Dispatcher -	Nil this period	
Master Builder - Cars -	Nil this period	
Master Builder - Motive Power -	Nil this period	
Master Builder - Prototype -	Nil this period	
Master Builder - Scenery -	<u>Division 4:</u>	David Whibley, Lesmurtle, WA
	<u>Division 6:</u>	Graham Cocks, Hallett Cove, SA Christiaan Werk, Osborne, SA
Master Builder - Structures -	<u>Division 7:</u>	John Arrowsmith, Bowen Mountain, NSW
Model Railroad Engineer - Civil -	Nil this period	
Master Model Railroader -	Nil this period 

Divisional Reports

Division 1

From Paul Rollason (NMRA Inc.-AR Division1 Superintendent)

August meeting:-

Meeting Attendance and Apologies:

28 members

2 visitors

28 apologies

AP Awards:

Nil

Special Mentions:

- Paul informed the Gathering of the recent passing of long-time modeller Mr. Ian Venables. He was a member for 35.2 years. Paul read out a piece written by Charles Page which summed up Ian to a tee. Thoughts and prayers to family and friends.

NMRA Regional Feedback:

NMRA National have now decided to provide the electronic magazine to all NMRA members free as part of their membership fee however this probably won't happen until late 2023 due to having to fund the magazine via advertising/sponsorship.

NMRA Division 1 Feedback:

Stream committee now had 3 meetings and planning to trial a day late this year or early next year to involve youth and their parents.

Large batch of NMRA Div 1 shirts and jackets now on order and still 2 weeks away.

Report:

The monthly gathering was hosted by Craig and Debbie Mackie's residence at Parkinson, Queensland.

There were 28 members in attendance and the gathering. The meeting was due to be telecast via Zoom, but due to the NBN not working in the area, there was no internet connection and consequently no Zoom recording.

Attached to the report will be the 4 Clinical Presentations made.

[Gone Home](#)

Paul Rollason (Div Super) advised the gathering of Ian Venables passing the week prior. A minute's silence was observed to pay tribute to Ian and for friends to remember the amazing individual and outstanding modeller Ian was. Ian was a NMRA member for 35 years and 2 months and a Life member. He was a member of the Modular SIG Group.

Paul then read out a tribute to Ian written by Dr Charles Page:

Ian Venables – graciously written by Dr Charles Page

Newer Division 1 members may not know that for many years Ian used to edit the Div. 1 newsletter, like many such editors he was continually short of articles for inclusion. This never really perturbed Ian as he would simply write them himself, often with a rather philosophical slant. I've included one of Ian's last newsletters, April 2018.

The final paragraph is, I think, a fitting memorial to Ian

So, we are a varied lot, probably more so than we have guessed. And as I always say, "There is a lot of knowledge within the NMRA-AR". Let's hope we can keep sharing it.

Ian was a member of a group that have running and operating days on my layout.

Ian's passion was scratch building rolling stock, especially freight cars: he built over 250 of them. He was a true master, he would always bring a selection to the running days, his skill level was unmatched. I had some new Intermountain box cars, which are noted for their detail, when I put Ian's cars next to them mine looked almost primitive by comparison.

None were freelance, they were all the product of extensive research, there had to be a prototype, often they were connected to his favourite prototype, the Louisville and Nashville.

His last project was being built for me, at one of our operating sessions I'd expressed an interest in Helium tank cars, the next time we met he had all the research and plans to make one. "I'm going to make that Helium tank car for you, it's probably the most complicated car that I've yet built". This was several years ago and each time we met at meetings or running days he would bring along the model for me to see his progress, he was often frustrated because his health issues kept getting in the way. He did almost finish it and the last time I saw him he was waiting for the custom-made decals. I was always apprehensive about accepting it because I knew how much work and passion he was putting into it. Now I'm sad not to have one of his scratch-built works of art, his work had *je ne sais quoi* (a quality that cannot be described or named easily).

Ian was also a respected ornithologist, birds and trains sometimes overlapped. I live next to the Seven Hills bushland and quite often when the group sat outside having lunch the far-off cry of an unusual bird would attract his attention. I was never sure if he would return to the layout or gallop off into the bush to confirm the identity of the owner of the warble.

May Ian now rest in peace and in no more pain and suffering. May he continue to enjoy his passion in the afterlife.

Late inclusion: - Also Gone Home was Craig Thistlethwaite who sadly passed away on 23 August 2022 at his home in Bundaberg. He was a NMRA member for 10 years and 10 months and an active member of the Miniature Train Club Gold Coast. His sudden passing is a shock to all who knew him.



Presentations & Clinics

Presentation #1 - Malcolm Jenkins MMR

Malcolm gave an amazing summary of "Timetable Based Operations" and how he uses this on his N Scale layout to simulate actual real-life operations from Adelaide to Tailm Bend.

Malcolm explained "Up" and "Down" trains and train numbers based on actual timetables and how he used these to make his own timetables. He uses a program called "J Train Graph" (free version and a program used by DB in Germany) and uses 6 to 1 fast clock to fix the time (scalable time). "J Train Graph" allows Malcolm to see if there were any errors in his timetabling. The trick is to balance "Up" and "Down" trains.



He mentioned that time-table operations is not for everybody but is a good way to structure operations, challenge people and have fun.

Presentation #2 - Al Wright

Presentation was a continuation from previous gatherings (Part 4) where Al Wright showed the progress of his 009 Fine Scale Miniature Kit (FSM) "Jeffries Point Stave and Handling Co. The model is of what was a distillery but later became a barrel house.

Al discussed:

- The importance of getting the roof correct for aesthetics
- Adding "view blocks" to prevent you seeing through the model and out the other side
- Consider sagging old building roofs but cutting a small elliptical edge off and pinch the roof whilst the glue sets.
- Choosing roof materials (shingles, tin, corrugated iron)

- Bragdon Powder (actual rust very finely ground)
 - o Used weather and add rust
 - o www.bragdonet.com
 - o Cost approx. \$37
- Ammo (dry brush paint) and uses a special very fine brush
- Finishing touches
 - o Using AK Interactive Slimy Grime - good for adding moss
- Aged, corrugated iron
 - o Use Ferric Chloride
 - o Eats away at edges to make realistic
 - o Caution - wear protective glasses and PPE
- Add roofing details
 - o Vents, water tanks, etc
 - o Dab of "burnt sienna" oil colour diluted
 - o Chimney
 - § Base coat Vallejo Red Brown
 - § Roof ladders
 - § Adding date plates
 - o Slicing tools
 - § RP Tools
 - § North West Short Line
 - § Purchase from Irresistible Forces (Opposite Hyperdome at Slacks Creek) They have RP Tools



Above: Al Wright presenting progress of his 009 Fine Scale Miniature Kit

Below: Al Wright's 009 Fine Scale Miniature Kit - "Jeffries Point Stave and Handling Co"





Presentation #3 - Arthur Hayes MMR

Arthur gave a short presentation on the "Pros and Cons of a mini(cordless) airbrush".

Arthur recommends Neat and Handy for their cordless mini brush and they are located in Melbourne.

- Cost \$81
- Double action brush
- Comes with 0.03mm needle
- 3 cups
- Adapter
- USB charger
- Works at 20psi
- Has an adjustable stop
- 20-30min run time
- Has 1/8" BSP thread
- Spare/additional parts
 - o Replacement needles 0.02, 0.03, 0.04mm for \$11 each
 - o Cleaning kit \$54
 - o Replacement air brush \$30
 - o Quick release adaptor and regulator \$22
 - o O-Ring (set of 4) \$14
 - o Can also get Compressor kit with 2 batteries \$97

Arthur suggested to use:

- SMS paints
 - o Acrylic lacquer
 - o Air brush ready
- PGC
 - o Acrylic lacquer
 - o Use with 50/50 paint and thinners
- Testors Dull Coat
 - o Air brush ready
- Vallejo model colour
 - o Use with 50/50 paint and thinners



Arthur Hayes MMR presenting "Pros and Cons of cordless spray guns"

- Thinner mixture

Vallejo Flow Improver, 30% thinners, 2-3 drops

Presentation #4 - Arthur Hayes MMR

Arthur outlined the Achievement Program (AP) for Motive Power which is a continuation of clinics around the AP and he talked about what was required to achieve the certificate.

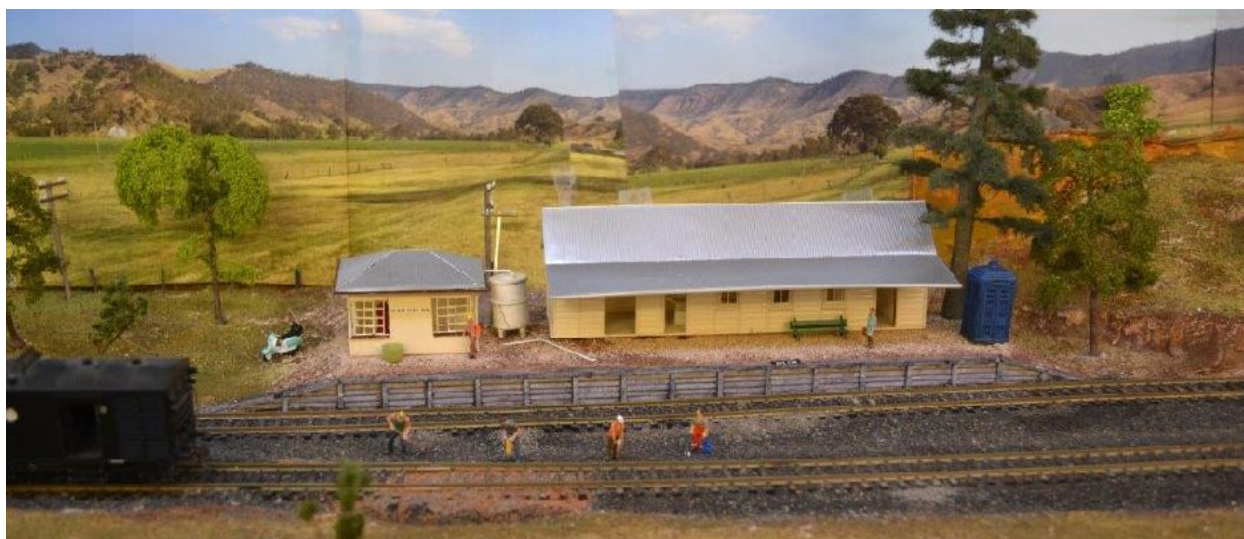
He commented that not many people do this certificate worldwide and this is one of the certificates that goes to the USA for sign off. Funnily enough it has the simplest of instructions.

- Build 3 models – 1 must be scratch built and all must operate
- Power trains for locomotives can be commercial
- The locomotive **MUST** be super detailed, or you won't get your points
- What is motive power? Anything that moves itself on track.
- Scratch built locomotive
 - o 90% of parts need to be scratch built
 - § Exceptions are motors, gears, lights, marker lights, couplings, bogies, brake fittings etc
- All 3 models need to achieve merit award of 87.5/125 points

Remember to take photos along the way

DIVISION 1 News

- STREAM subcommittee has had a further meeting since last meeting and are planning a trial day late 2022 or early 2023
- Shirt and jacket orders have been placed – arrival early September. Next order cut-off date is 31 Oct. Jackets \$80 and shirts \$35.
- We have NMRA track gauges for sale in both N and HO scales
- As a result of the National Meeting held in the USA last week and after some strong argument around the table, ALL NMRA members to receive electronic magazine



starting late 2023. They are working through a process of how they will fund the newsletter and they will do this via advertising

- Planning a visit to the Gladstone Model Railway Club in February/March 2023
- Request for volunteers) ambassadors and clinicians) for the Redland's Model Railway show on 27-28th Aug
- Reminder for the New England Model Railway Convention I Armidale on 22-23rd October
- The bus trip to Warwick on 12-13 November is now definitely going ahead.
- Div 1 Christmas Function now 26th November at the Monier Hotel, Darra
- Buy and Sell Day being planned for Feb/March 2023
- Still require hosts for 2023 program - please contact Glen McCarle

Show and tell:

- Kevin Frost showed us a 3-piece light kit from Bunnings and is rechargeable
- Craig Mackie had two items: -

Use of old model boxes (repurposing/recycling)

- Use styrene to shape for wagon loads
- Acetate sheet for windows

Craig showed us his Platform Printing figurines

- Track side workers (\$25 for 10 people)

Craig showed us how he painted them up. These models are quite detailed and can be printed in different scales



Below at photos of Craig Mackie's layout





Division 1

Paul Rollason (NMRA Inc.-AR Division1 Superintendent)

Report from Bob Tisdall

September 24th meeting:-

Meeting Attendance and Apologies:

24 members

0 visitors

38 apologies

AP Awards:

Nil

Special Mentions:

Recent passing of Craig Thistlethwaith

NMRA Regional Feedback:

Nil

NMRA Division 1 Feedback:

Large batch of NMRA Div 1 shirts and jackets have now arrived for payment and collection.

Report:

The Gathering was hosted by the Logan District Model Railway Club at their clubhouse. Thirty-eight people signed the attendance sheet, and all were comfortably seated after a layout was moved to the outside (just one example of the excellent support we received).

The team at Logan were welcoming and went "beyond" to make this an interesting visit. The BBQ lunch was excellent and special thanks are due to the two "chefs" who produced gourmet burgers with all the trimmings.



The club has two permanent layouts in HO and N gauge, the subjects covered by the books in the library are wide ranging and the availability of 3D printing equipment is impressive. Have a look at their website at [Home \(ldmrc.com\)](http://ldmrc.com)

LIGHTING WITHOUT WIRING

Phillip Flynn's presentation of the fiber-optic lighting system was comprehensive. Phil brought along a diorama that was set up using the Dwarvin lighting system. This added much to the comprehensive slide show. The model illustrated how this illumination system works and the flexibility that it offers for modelers wanting to set up for night-time simulations.

The ability of the modeler to retrofit lighting into existing structures would be a very useful feature for many like me have great ideas five minutes after you think you are done and dusted.

009 FINE SCALE MODEL

What a journey, this was the last of five episodes, each presenting the progress and techniques used to produce a super detailed structure that would grace a layout.



Al Wright's work sets a high bar for us all. This presentation moved through the final stages of the construction and concentrated on the "set dressing" aspects of the build. The idea of telling a story through the placement of actors and props illustrated the importance of imagination as well as technique. Al talked through the challenges presented by the build process and responses to the various difficulties that were met.

3D PRINTING CLINIC

The team from LDMC presented a comprehensive clinic making the art of 3D printing an approachable subject. Aubrey Hughes, supported by Ron Daniels, led the discussion and covered aspects of the different types of printers and their capabilities, designing models from scratch and the sources of readymade designs, the strengths and pitfalls of the technology, and much more.

Aubrey told me that this presentation was distilled from a workshop that lasted all day, so it is not surprising that it was information rich. For me, two issues leapt out; the effect of rescaling a model on wall thickness and the relationship of printing times if the model is tilted thus requiring many more layers.

An interesting discussion was had concerning the use of 3D printed artifacts in the AP awards assessment. Printing a download design would be a kit build, whilst creating the CAD design from and printing a unique design would count as a scratch build (this is my recollection and Arthur Hayes is the authority on this).



AP AWARDS AND HOW TO GET THEM

Arthur Hayes described the AP Awards scheme as it applied to rolling stock. This was the 4th lecture on NMRA awards scheme and discussed the kind of details that separate models capable of passing or exceeding the standards required to achieve certification. The number of artifacts and their variety was clarified (and no, you cannot replicate the same truck/wagon/car eight times).

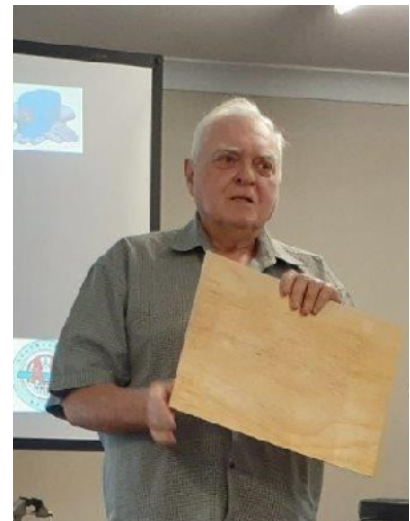


Arthur emphasized the importance of getting details to match the target scale of the model. Examples of over sized ladders and handrails were given to illustrate this point.

SHOW AND TELL



Left: Ken Edge-Williams - Ken spoke about tools he uses to cut fine timer pieces with.



Right: Richard Kontos - Richard spoke of the various timber products he produces.

DIV 1 BUSINESS

Next Meeting - 15/10/2022 Glen and Vesna McCarley's 7 McPherson Roads Sinnamon Park 4073.

Polo Shirts and Jackets have arrived - Paul has them available for collection

New England Model Railway show 22nd - 23 October 2022

Warwick Weekend away 12-13th November 2022

391 Div 1 members - let's get beyond 400.

THANK YOU

Thanks are due to the team at LDMC, Glen McCarley for logistics support and Artur Hayes for the photography.....

Division 2

Stephen O'Brien (NMRA Inc.-AR Division 2 Superintendent)

September meeting:-

Meeting Attendance and Apologies:

8 members

1 visitors

15 apologies

Report:

The September meeting was an idyllic day for members to gather at the home of Rob and Jenny Anderson at Bemboka NSW. Somebody said the weather in Canberra/Queanbeyan was miserable. For just the day or a weekend this was the place to be.

Eight members made the journey and Rob's neighbour Craig from over the back fence attended. With an enjoyable lunch taking in the view from the deck we could not have asked for a better time.

The meeting was held with some questions asked and some answers that need more clarification given. Show n Tell, Rob's layout philosophy and Craig's Blog and Youtube videos were presented.

Members enjoyed visiting Rob's shed to see layout progress and watch trains running the tracks. A layout with no duck-under, just walk right in.

Afternoon tea was served to round out the day and we can thank Rob and Jenny for hosting another enjoyable meeting that was outside of Canberra.

Greater detail and more photographs of the meeting can be found in the latest issue of The Flimsy, Division 2's newsletter on the Australia Region NMRA website.....[!\[\]\(b4eeff342f60cc7bcd67d869b4fedca2_img.jpg\)](#)



Division 3

Peter Kendall (NMRA Inc.-AR Division 3 Superintendent)

Report Provided by Peter Kendall and Rod Hutchinson

September 11th Meeting:-

Meeting Attendance and Apologies:

8 members

4 visitors

5 Partners

ARC Report:

Nil

Special Mentions:

Recent passing of Craig Thistlethwaith

NMRA Regional Feedback:

Nil

NMRA Division 1 Feedback:

Large batch of NMRA Div 1 shirts and jackets have now arrived for payment and collection.

ARC Report:

Apologies for September meeting from Div 3 Superintendent Peter Kendall, Gavin Hince, Peter Batten, John Droste, Ron Bennell, Alan Ogden.

Grant McAdam filled in as A/Superintendent whilst Peter Kendall was overseas.

Grant reminded us of ARMMEX 22 Expo. Macedon Ranges MRC in association with the Cottage Industry Consortium present a small manufactures expo. Saturday 17th & Sunday 18th 2022 at the Masonic Hall, Yaldwyn Street Kyneton Saturday 10 to 5, Sunday 10 to 4. \$5:00 dollar flat rate entrance fee. This is an expo of small manufacturers that produce unique items for the model railway industry that will be open for the modellers to come along and see first hand how things are done, hands on experience as well as clinics on modelling techniques.

Meeting Report:

17 Modellers, Partners & Guests attended the September meeting held in Tarneit, West of Melbourne. Geoff Truman hosted the event at the Palm Lakes Resort Lawn Bowling Club. This would have to be our premier meeting location which had plenty of room, a well presented kitchen and barbeque area.

Geoff has an association with Broad Gauge Models and his preferred scale is HO Victorian Railways outline. Geoff invited residents of Palm Lake Resort Model Railway Group who presented us with a plethora of OO scale Hornby model engines collected over many years and Garden Rail size locos and rolling stock, some fitted with battery on board radio control. One of the most interesting locomotives was a steam driven locomotive in which the throttle was operated by a servo using Radio Control. A truly remarkable achievement.

Our host, Geoff, presented a collection of early VR rolling stock produced by milling plastic or 3D printing. A wonderful collection of the not so common early VR rolling stock.

Grant McAdam, filling in for Peter Kendall as A/Superintendent reminded us of ARMMEX 22 Expo.

Macedon Ranges MRC in association with the Cottage Industry Consortium present a small manufactures expo., Saturday 17th & Sunday 18th 2022 at the Masonic Hall, Yaldwyn Street Kyneton Saturday 10 to 5, Sunday 10 to 4. \$5:00 dollar flat rate entrance fee. This is an expo of small manufacturers that produce unique items for the model railway industry that will be open for the modellers to come along and see first hand how things are done, hands on experience as well as clinics on modelling techniques.

He presented our host with the NMRA thank you plaque.

Models on Display

Geoff Truman: Victorian Railways HO scale



AMK models milled VR
"Norman" Car

AMK models milled VR
"VAC" Car



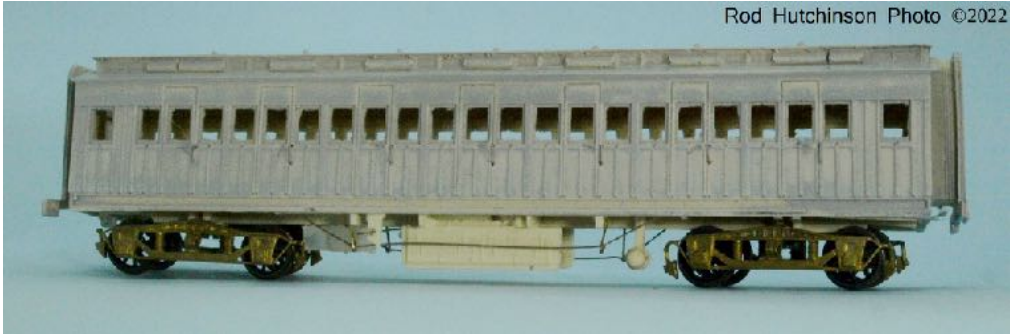
Kitbash "BCE" car

Scratch "Post Office" car





VR Casts "ALX" car carrier



VR Casts "BV" car



VR Casts "XY" car




VR Casts "YZ" car

Outback Model Co.
Factory Flat



Peter MacDonald: MRT
Prints O scale painted
animals



Visitors: OO scale Hornby Loco Collection
Garden Rail Locos & rolling stock.....

Turn to page 77 for the October meeting report for Division 3

Division 4

From Frank Godde, MMR® (NMRA Inc.-AR Div4 Superintendent)

July Meeting:-

Meeting Attendance and Apologies:

7 members

3 Zoom

2 apologies

AP Awards:

Nil

ARC Report:

Nil

NMRA Regional Feedback:

Frank mentioned the ARC zoom meeting .


NMRA Division 4 Feedback:

Nil

Meeting Report:

We went to Dennis Turner's home in the Mandurah area. Frank discussed the advertising flyer that is being prepared for Div Four. It was decided to use the Div One format but alter it to reflect Div Four. We will use photos of previous work displayed at shows, and some permission will need to be gained here to use some of the photos.

After coffee and nibbles, we moved, with the zoom link on Dennis' laptop, to the train room. This has been air conditioned since our previous meeting. We stood on the raised floor at the rear of the long layout to view what lay before us. Dennis models in N scale and there was a lot of track, yards, and cardboard cut outs of impending industry placed on the layout. Dennis gave us a power point presentation on the reason why he is doing all this work. He is keen on the railroads of Denver and the surrounding areas. The meeting was viewed by means of a larger screen fixed to the wall, so that the zoom members could also be included. It was an interesting meeting and the first one to include a zoom connection. Thank you to Dennis and Duncan for getting the link organised. It is an easy thing to say, but it requires a fair bit of effort and understanding of the processes to make it all possible

The article which Dennis has written on Denver's railroads has been printed in a N scale magazine with photos and if you are interested, please contact Dennis and ask for it to be sent to you.

Division 4

From Frank Godde, MMR® (NMRA Inc.-AR Div4 Superintendent)

August Meeting:-

Meeting Attendance and Apologies:

7 members

1 Zoom

1 apologies

AP Awards:

Nil

ARC Report:

Nil

NMRA Regional Feedback:

Nil.

NMRA Division 4 Feedback:

Nil

Meeting Report:

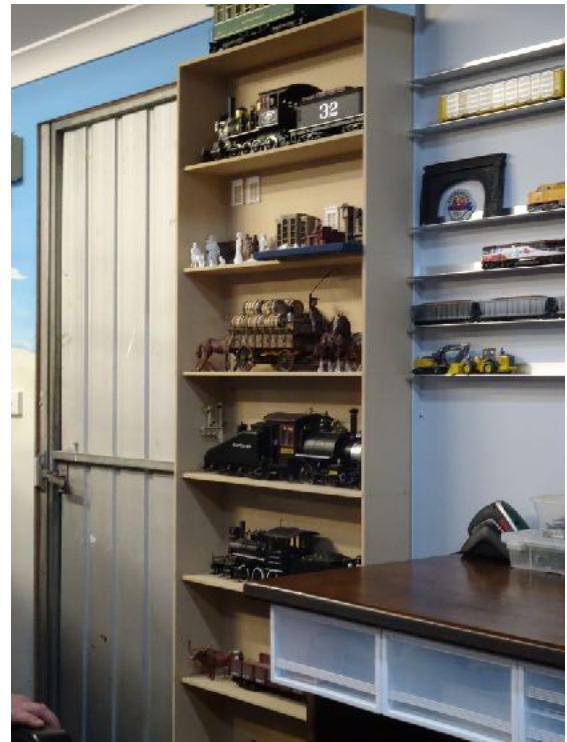
We all went to Dave's place for this month's meeting, there was eight of us which included one on zoom. All of the members sat inside the new train room that is attached to the large train room. Dave had mentioned that he cleaned the track just for the occasion. Linda, Dave's wife, brought in the goodies, drinks were served and on we went. Frank outlined what was happening for the rest of the year.

Peter Flower sat in with us via zoom and later on was showed through the large



train room by the use of an extension cord, so he also was able to see the trains running. There were quite a lot of trains with sound on the track. While all this was going on Dennis filmed the running trains on his phone. We intend to make a Youtube presentation to show the workings of division four here in Western Australia.

We eventually took the laptop outside to show off the two G gauge modules



that were set up on the lawn there. The new members were justly impressed and Matt told us that his forth coming train room was up and beaming lined inside. Michael mentioned that the two of them were looking for a package deal for a home. It was a very good August meeting.

Show and Tell:

Frank showed the new windows printed by Dave on the 3D printer which would be ready for the next month's meeting, the topic will be on "back drop buildings".

Alan B. brought in his newly acquired daylight 4-8 -4 still boxed and told us that he was in the process of negotiation for the coaches to go behind the locomotive.....

Division 4

From Frank Godde, MMR® (NMRA Inc.-AR Div4 Superintendent)

September Meeting:-

Meeting Attendance and Apologies:

7 members

2 Zoom

1 apologies

ARC Report:

Nil

NMRA Regional Feedback:

Nil.

NMRA Division 4 Feedback:

We discussed what we were going to plan for the 2023 AMRA show and the paper work was going to be planned by Frank for all to put in comments.

AP Awards:

David Whibley received his first AP certificate for structures.

Clinic:

Frank gave his talk on materials that he had put together over the years that you would find around the house and were of no cost, followed by the topic advertised for the meeting, backdrop buildings.

These were shown to the group and also the two members on zoom. Matt said that he had never thought of doing



Dave Whibley receives his First AP certificate, being Master Builder - Structures



Back Drop Building, paster walls on polystyrene

buildings that way but had already bought the bag of plaster in anticipation.

Frank Godde MMR with the aid of some of his back drop buildings showed and talked about the process of building and putting them together. Using latex rubber and a plastic wall from a Walther's kit made a rubber master which was then filled with casting plaster and left to dry. Once he had quite a few of them and using gap filler glued

them to a polystyrene block, then they were coloured with a wash and left to dry. The windows were painted black and signs,



MDF building with concrete beams before paint



Cardboard and redwood planking over polystyrene

names, water tanks and bill boards are then also fitted so finishing off the building.



Styrofoam and plaster walls

The other ones were made from card and veneer but also using Grandit windows.

ON30 people fill the seen and poetic licence is always used.

Show and tell:

Alan brought to the table his newly acquired daylight coaches but needed three hands to take them out of the box, so the item was passed around init's box....**M**

Turn to page 79 for the October meeting report for Division 4

Division 5

Philip Sharpe (NMRA Inc.-AR Div5 Superintendent)

Regarding September / October meetings:-

There has been no report received for the September / October period from Division 5.
.....**M**

Division 6

From David Orr (NMRA Inc.-AR Div6 Superintendent)

August meeting:-

Meeting Attendance and Apologies:

17 members

2 visitors

28 apologies

AP Awards:

Graham Cocks, AP Scenery & Golden Spike.

ARC Report:

Nil

NMRA Regional Feedback:

See comments in report.

NMRA Division 6 Feedback:

See comments in report.

Report:

Seventeen members gathered at Adelaide Model Railroaders Clubrooms for our August meeting.



The Gathering



David Orr welcomed all to the meeting, especially Graham Cocks, one of his rare attendances, Jeff Barclay, President of Adelaide Model Railroaders, his 2nd attendance, Kent Henschke, Peter Kirkland and Tony Mikolaj, members of Adelaide Model Railroaders, their 1st attendance and Andrew Beattie, our newest member. Welcome all. David presented the Meeting Host Plaque to Ken House, Adelaide Model Railroaders Treasurer.

In the absence of our stand-in Treasurer, Ron Solly, David advised the meeting of our current financial state. Ron had a hip replacement last Wednesday and is doing well.

David advised the meeting that he had nothing to report at ARC level since our last Div 6 meeting. The next ARC meeting is next Tuesday, the 16th.

AP Asst Manager, SA, Ray Brownbill, had great pleasure in presenting Graham Cocks with his AP Certificate for Scenery and his



AP Certificate for Golden Spike.

Librarian Michael Robinson advised the meeting that the May 2022 and June 2022 NMRA Magazines are now in the library. This will be our last as the Division has elected to cease the subscription. Michael also advised members, especially those attending for the 1st time, of the substantial DVD library Div 6 owns and encouraged members to borrow. A call or email to Michael and he will be very happy to mail you a DVD or 2. Newest member, Andrew Beattie, advised that he would like to donate some DVDs to the library. Thanks Andrew!

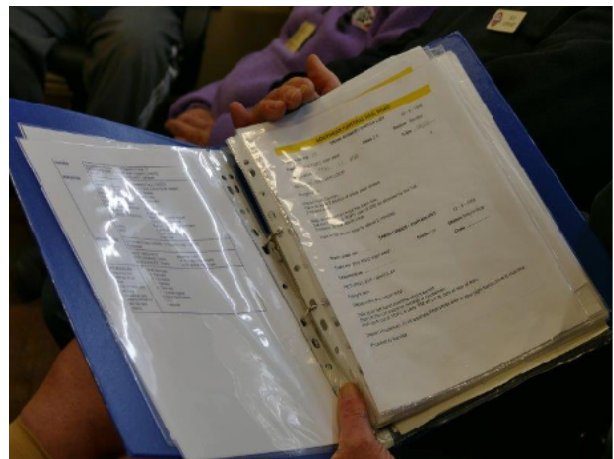
David reminded members of the large number of buildings, bridges and track from the Ray Applebee estate, stored in Ron Solly's shed. Members are encouraged to select whatever they need. Ron says that if you go in the shed, you're not allowed to leave without taking something. Ron would like his shed back!

With the official part of the meeting over, it was time to go round the group and find out what everyone has been up to.

Kent Henschke showed the group how to re-constitute Bullfrog Snot.

Jeff Barclay showed the group some 1974 Model Railroader scale conversion charts. Kent suggested that they could be scanned and shared via email. Andrew Beattie advised that many libraries offer this service and was coerced into taking this on.

Ken House hasn't done much with his home layout but is looking for operators. Ken has been busy with projects for Adelaide Model Railroaders. A car float for the port has



taken some of his time but most time has been spent designing operating sessions for Adelaide Model Railroaders, using switchlists. A sequence lasts 3 months. Ken showed the group some of his handiwork.

Michael Robinson has been busy building white metal kits and uploading videos to Youtube.

Ron Dunkley has been re-wiring part of Michael Robinson's layout. The two of them are redesigning the upper level of Michael's layout and building in auto reversing loops.

Peter Kirkland has a new shed! He's filling it with a new modular layout he's building.

Andrew Beattie gave the group a little of his background. Andrew's interest in model railways started when he was 8 and, like a lot of us, he started with Hornby Dublo. Some years later, he changed over to US HO scale. Like most of us, life gets in the way and model railways goes in to the background. After immigrating to Australia, he re-kindled his interest 8 years ago. But this time, his interest is much bigger. He now runs a garden railway in Fn3 using BlueRail.

David Teague continues to work on David's Pebble (see last month's report) but now he's on to a bigger version and is looking for a name. Something catchy and humorous.

Jane Robinson has taken a month off anything trains and is just being a Mum. She's joined a gardening club. Did someone say traitor?

Graham Cocks is busy modifying points/turnouts/switches to take bigger rolling stock.

Bob Bevan now has a new quarry in his layout, thanks to Ray Brownbill!

Ray Brownbill has been working on Bob Bevan's layout, not only putting in a new quarry but also a new reversing loop. On his own layout, he continues with scenery. Ray advised the group of some Bowser wheelsets he purchased which are a replacement for Steam Era wheelsets, something becoming hard to find. These are a scale 26' in length



and come 20 in a packet. These came from an NMRA colleague in Sydney at substantial saving. Get in touch with Ray or David Orr if you'd like more info.

Vern Cracknell advised the group he was at the recent Stawell exhibition. He took the "Running the Gauntlet" layout. He was presented with People's Choice 2nd prize. Well done Vern! This was the layout's 9 exhibition and it's last. Vern's helpers are finding it difficult to move. The layout has

now been dismantled and part of the layout, a bridge, was there on display for anyone that wanted it to take it.

But no flies on Vern, he's already started his next layout - a G Scale micro layout! This will be using Vern's short wheelbase rolling stock.

To meet Vern's needs, he has the task of bending the track to get the radius he requires. So in his usual style, Vern now has a fabulous track bending device. He's very impressed with it.



Tony Mikolaj has been developing the

signalling circuitry in use on the Adelaide Model Railroaders club layout. Tony demonstrated the working signals.



After viewing the working signals, the group enjoyed afternoon tea.

Our next meeting will be September 16th at David Orr's, 53 Rawlings Road, Modbury North....[M](#)

Photos: Here are some photos of Adelaide Model Railroaders club layout.

Adelaide Model Railroaders layout is very extensive and you can see more by searching for Southern Central RR or Adelaide Model Railroaders on Youtube.



Division 6

From David Orr (NMRA Inc.-AR Div6 Superintendent)

September meeting:-

Meeting Attendance and Apologies:

14 members

AP Awards:

Jane Robinson:- Association Official

NMRA Regional Feedback:

See Comments in Report.

NMRA Division 6 Feedback:

See Comments in Report.

Report:

Fourteen members gathered at David & Maggie Orr's home for our September meeting.



The Gathering



David welcomed all to the meeting, especially new member Rowan Lee. As David was the host, the gathered members insisted that David present himself with the Meeting Host Plaque.

In the continued absence of our stand-in Treasurer, Ron Solly, David advised the meeting of our current financial state. Ron's hip replacement is coming along very well, Ron expects to be driving next week.

David advised the meeting of the latest ARC meeting. Among the items discussed was the NMRA AU website – David recommended everyone have a look at it remembering it's a work in progress and whilst the main structure is there, details have to be added. There's been a change in the management of the website and the AU Regional President is investigating alternative possibilities.

AP Asst Manager, SA, Ray Brownbill, had great pleasure in presenting Jane Robinson with her AP Certificate for Association Official, a little belated but most deserved..



Librarian Michael Robinson advised the meeting that David Orr had gifted a collection of DVDs to the library, and was pleased that at least one member was taking advantage of the library.

The meeting was advised (or reminded if someone already knew), that the SARMA swap meet is coming up on the 18th of this month at the Avenues College, Windsor Gardens, commencing at 10am and the Limecoast Model Railway Group is holding an exhibition as part of the Geltwood Festival on October 15th & 16th in Millicent.

With the official part of the meeting over, it was time to go round the group and find out what everyone has been up to.



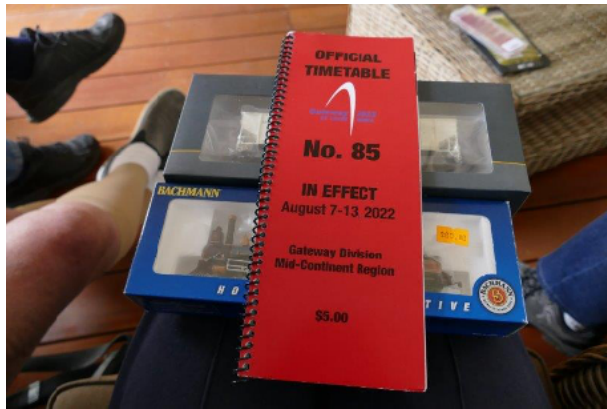
Paul Wright had been painting and decaling U and T vans.

Jane Robinson has been making plaster moulds for Michael's layout. Jane asked the group on the best method of fixing MDF

fascia to the layout's timber frame. PVA glue was the consensus.

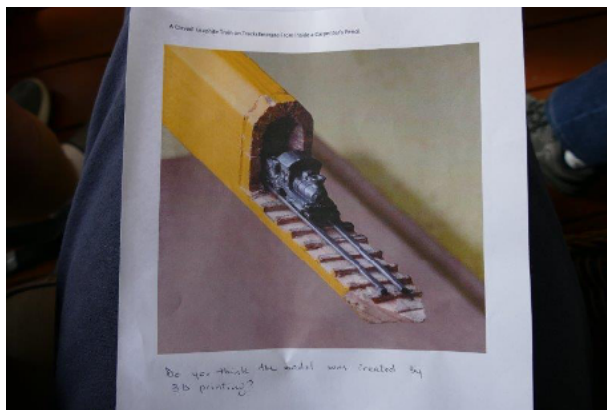
John Prattis showed the group a poster of Steam Locos of NSW he received as a Father's Day gift. John also advised that the last part of the Eskbank layout has now gone back to Lithgow to be part of the museum. John attended the NMRA Convention in St Louis, met and chatted with a number of well-known modellers, went on a number of tours including one to Micro Engineering and returned with a large collection of souvenirs, including the Convention Car.





Vern Cracknell showed the group a locomotive "emerging" from a carpenter's pencil. Vern discovered that an airbrush was full of gunk so he placed it in a vegemite jar with some water-based turps. The results were astounding!

Vern also showed the meeting a plan of his new coffee table G scale layout. Vern went on to tell the meeting about an upcoming exhibition he will be exhibiting his new G scale layout at being put on at Millicent by the Limestone Coast Model



Railway Group as part of the annual Geltwood Festival. The poster and program for this festival and exhibition will be included with this report. Arty O'Connell, one of our NMRA Division 6 members in the South East, has created the



Limestone Coast Model Railway Group after moving there from Adelaide. Arty is well known in Adelaide as he was the Convener of the Adelaide Model Railway Exhibition for a number of years.



Bob Bevan continues to progress his layout with the aid of a couple of friends. He has been selling some of his model railway collection in an attempt to reduce his inventory and has now added even more to the "for sale" collection.

Ray Brownbill has continued his assistance with Bob Bevan's layout. On his own layout, Ray has been doing more scenery and fixing locos, especially handrails, which seem to come off after an operating session. Rocket glue is the only glue he's found that will glue Delrin plastic.

Jeff Barclay has found some books and other paraphernalia of the Southern Cross Model Railway club. This group was formed in the 50's as the Australian area for the NMRA. These records will be offered to the SA Museum.



Michael Robinson showed some new items from Hobby Habit and advised that the new turntable was installed in his layout, the bridge was up and power had been connected to the new parts. He also reminded the meeting of the upcoming BRMA Convention beginning on the 30th September.

Rod Stewart has started connecting dropper wires but was having trouble soldering them on to the track. That led to a discussion on what wire to use for droppers, how to prepare the track, ways of keeping the soldering iron clean.

Ken House advised the group that the 00 collection that he advised the group about has been sold. He's also aware of a collection of marine ply sheets available which would make excellent baseboards. Ken then showed the group an Austrains bogie he's having trouble with. A loco which has been trouble-free for ages has developed an issue. After



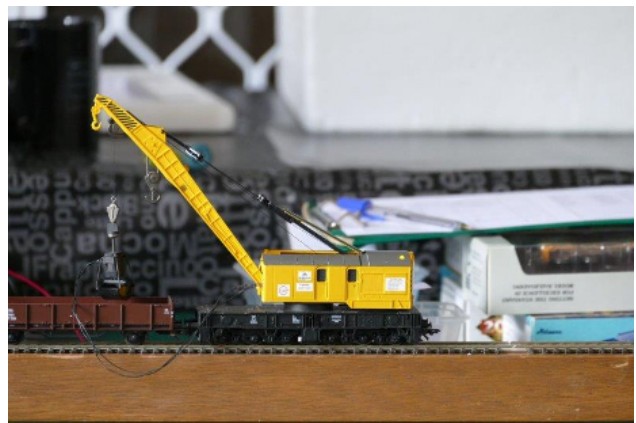
dismantling the loco, Ken has found that one of the bogies is binding haphazardly and is hesitant to dismantle the bogie as his experience is that every time he does, he breaks a clip! His new title is going to be "Ken of the Broken Clip".

Rowan Lee, one of our new members, gave the group a little of his background. Rowan was formally a member of Division 7 of NMRA and a member of the Hills Model Railway society. His interest in model railways started when he was 10 when his father took him to his 1st model railway exhibition. In his own words, Rowan was hooked. Rowan's

interest is with Canadian Pacific and Canadian National and he has 2 partially built layouts, 1 in N scale and 1 in H0.


David Teague continues to work on David's Pebble (see July's report) and he's now preparing for the exhibition at Millicent.

Ainslie Brittain has re-discovered a Roco Digital Crane, which had been hiding in a box for many years. Ainslie couldn't work out why it was there until he tried to use it. Then he remembered! But, after spending some time with it, fixing this and fixing that, he was able to get it working again. And it's very impressive. It has 3 motors in it, allowing it to turn, move the jib up and down and open and close the bucket. And unlike the usual experience where something that works at home refuses to work when displayed, this worked! And worked well!



The meeting was asked if we wished to get together for a Christmas Lunch. The plan was to repeat what we did last year and have a Christmas Lunch on the same Saturday as the November meeting. Unfortunately, the November meeting was going to be held at the residence of Neil, Liz and Casey Tonkin, but circumstances have changed and Neil advised that they weren't able to host it. So the November meeting will now be at Ron Solly's at Evanston Gardens. Seven of the members present have indicated that they would participate in a Christmas Lunch, five indicated possibly. Given those numbers, we will go ahead with a Christmas Lunch on the 12th November, before going on to the meeting at Ron Solly's. Current suggestions have been the Old Spot Hotel, the Smithfield Hotel and the Southern Hotel in Gawler. I will need to know numbers very soon to be able to make a booking.



That ended the meeting so we all partook of afternoon tea. Our next meeting will be October 8th at Ray Brownbill's, 4 Acorn Place, Blakeview.....

Division 6

From David Orr (NMRA Inc.-AR Div6 Superintendent)

October meeting:-

Meeting Attendance and Apologies:

12 members

AP Awards:

Ron Solly - Association Official

Jane Robinson, Michael Robinson, David Teague, - Achievement Program Badges

ARC Report:

Nil

NMRA Regional Feedback:

See comments in report.

NMRA Division 6 Feedback:

See comments in report.

Report:

Twelve members gathered at Ray & Gael Brownbill's home for our October meeting.



The Gathering



The birthday boy!

David welcomed all to the meeting, and presented Ray with the Meeting Host Plaque.

The gathered members then wished Ron Solly a happy birthday.

David advised the meeting of the current ARC activities. Members are encouraged to submit articles for the NMRA Magazine. Preference will be given to articles other

than HO and from Divisions outside of the US. Wayne Eagle is now the contact for anything to do with the AU website.

Treasurer, Ron Solly, advised the meeting of our current financial state.



Left:- AP Asst Manager, SA, Ray Brownbill, had great pleasure in presenting Ron Solly with his very belated but most deserved AP Certificate for Association Official.



Right:- Ray also presented NMRA Achievement Program badges to Jane Robinson, Michael Robinson and David Teague.

Librarian Michael Robinson advised the meeting that he was still in the process of updating the library with the recent donations.

A question was asked about the subject of modules. The meeting was informed that it was still the hope that modules would be built with at least 3 members interested.

On the subject of the Adelaide Model Railway Exhibition for 2023, the meeting was informed that NMRA will attend. At this stage there are 23 layouts and we'll be using 3m aisles again in lieu of 4.5m aisles. Monday's attendance at this year's show was substantially up on previous years, some feel due to the television news coverage on the Sunday evening news. Attempts will be made to have TV news coverage again, this time hopefully for the Saturday evening news, thereby gaining greater attendance for both Sunday and Monday.

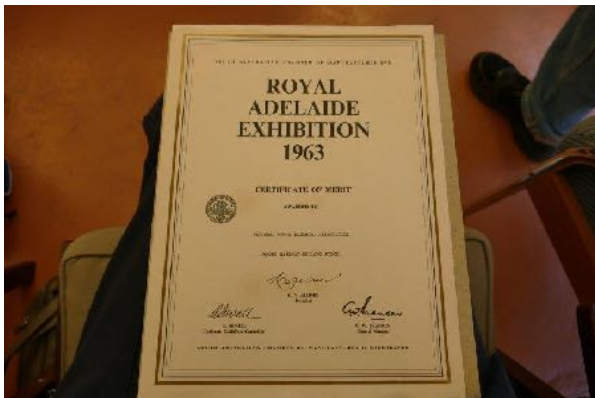
Division 6 Christmas Lunch will be held at 12 noon on the 12th November at the Southern Hotel, Murray St, Gawler. This will be followed by our November meeting at Ron Solly's, 9 Grey Cres, Evanston Gardens.

David Teague advised the meeting of an annual exhibition held near the end of November at St John's Church, Salisbury, the exact date and location to be advised. The meeting was asked if Division 6 would be interested in attending with NT Junction. This would be a donated event, ie Division 6 would receive no financial gain from this event but rather the opportunity to present NMRA and encourage membership. Five members present at the meeting indicated their willingness to attend, depending upon the date. More details will be forthcoming.

With the official part of the meeting over, it was time to go round the group and find out what everyone has been up to.

David Orr started the "round-the-group" off with a showing of some of the locos from the deceased estate of ex NMRA member, Ray Applebee. Some of these locos are quite spectacular, but suffer from mishandling with many small parts missing. David has been able to source some of the parts from Walthers. Now begins the delicate task of

refitting. The locos were presented in a wooden storage/carry box which David had repaired and a duplicate, empty box was offered for sale.



Left:- An item of great interest was a certificate discovered in old NMRA paperwork which shows NMRA's attendance at the Royal Adelaide Exhibition of 1963. This will be laminated and stored.

David Teague advised the meeting of his attendance at the Loxton exhibition as a member of the South Australian N Gauge Society. Apparently, the exhibition was very successful, Loxton never having seen a layout as large at SANGS at any of the previous shows. The meeting was asked if

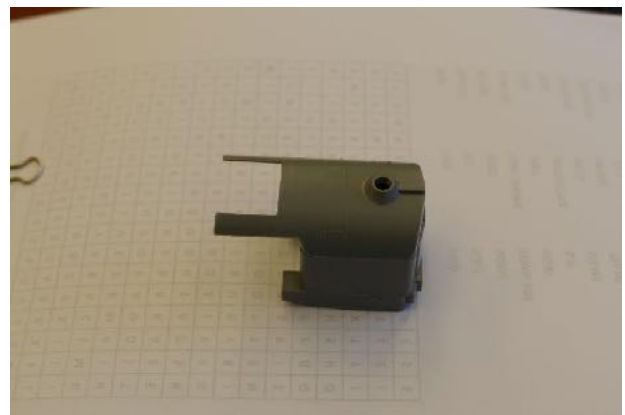
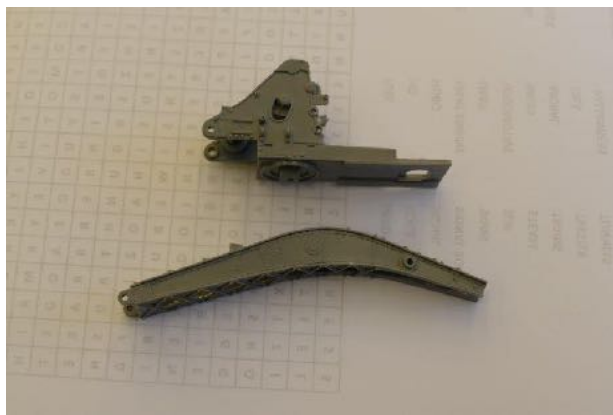
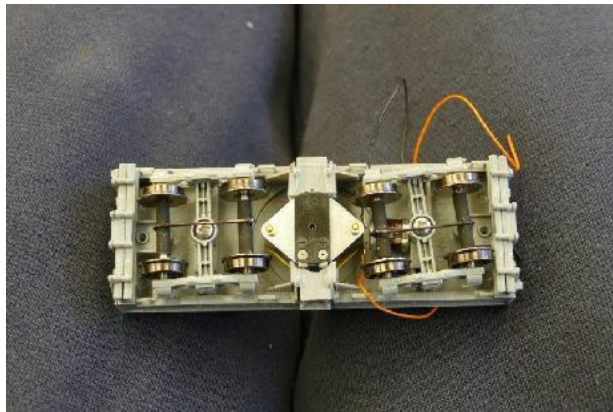
there was any interest in Division 6 attending such shows and an affirmative response was received depending upon dates, times, costs, etc.



After Ainslie Brittain's "fun" with a Roco crane (see last month's report), he was approached by John Prattis if he would consider



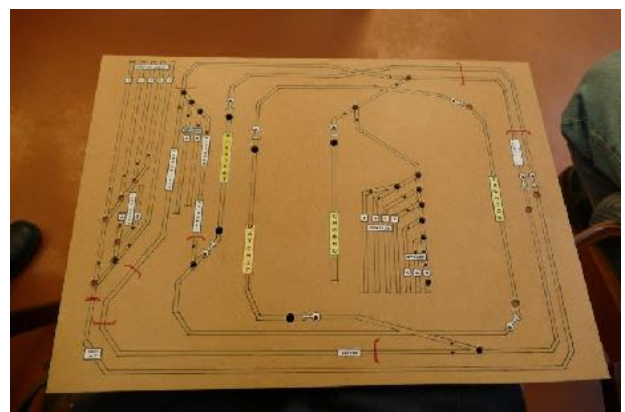
assembling and motorising an HO Tichy crane for John. Before Ainslie could run away, he was trapped. The idea is to emulate a NSW accident crane.



Ainslie related his efforts to date to the group, who watched and listened in amazement! So many small parts, 3 motors, decoders and more electronics. At the end of Ainslie's discourse, he was asked if he'd like to do an N scale one!

Rod Stewart is progressing with his wiring albeit slowly.

Ron Solly has been finishing some track modifications he started some weeks back but his greatest achievement has been combing his 2 dispatcher panels into 1. One of his operators "suggested" that 2 panels was 1 too many and to have 1 in front of the



operator and 1 behind the operator made operating difficult for newbies (Ron has for many years rotated his operators around the various positions but now includes the

dispatcher's position). Now comes the task of wiring that new panel. It may seem otherwise but Ron is very appreciative of the opportunity to tidy up his wiring.

Vern Cracknell will be attending the model railway exhibition put on by the Limecoast Model Railway Group as part of the Geltwood Festival on October 15th & 16th in Millicent. He will be taking Rosemount 2. Vern also showed the group some green extruded polystyrene, available from Bunnings, which can be worked very easily. It cuts

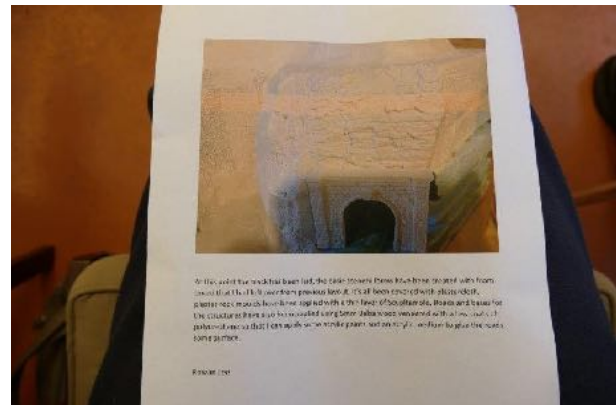
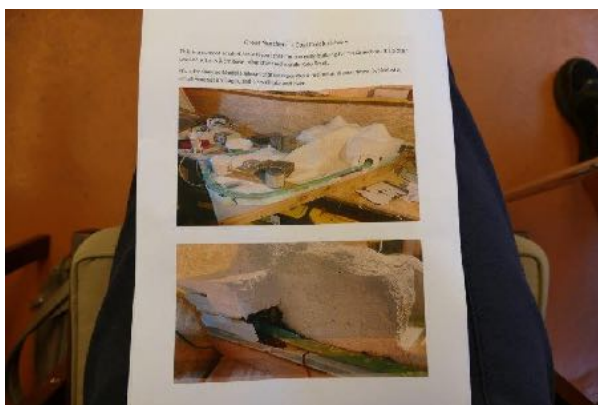


and is shaped very well, with none of the mess one normally gets from white polystyrene. Apparently, large sheets can be obtained from SA wall Systems. Vern also showed the group some information about "Doctors Without Borders" and their efforts to move people out of Ukraine with trains.

Peter Jackson has been discovering more features about JMRA and is now playing with Wi Throttle.

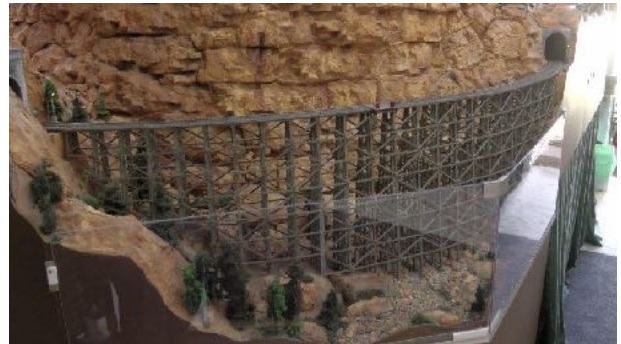
Bob Bevan tells us he's plodding along slowly. He's been having informative discussions about wiring and is in the process of determining where to place his panels. Recently, Bob purchased a couple of Auscision SAR 600 class locos and he's been having no end of trouble with one of them. Bob uses a Lenz system so Ray Brownbill who also uses a Lenz system took the troublesome loco to his place and eventually got it to work. But back at Bob's, it reverted to its old tricks. So it's gone back to Auscision for the 2nd time.

Rowan Lee is building a small N scale layout for his grandson.



Ray Brownbill has continued his assistance with Bob Bevan's layout. On his own layout, Ray has installed a protective screen near his trestle. At that point, the layout is below


waist height and he's found that after an operating session, trees near the trestle have



been "uprooted". And he's continuing with his scenery.

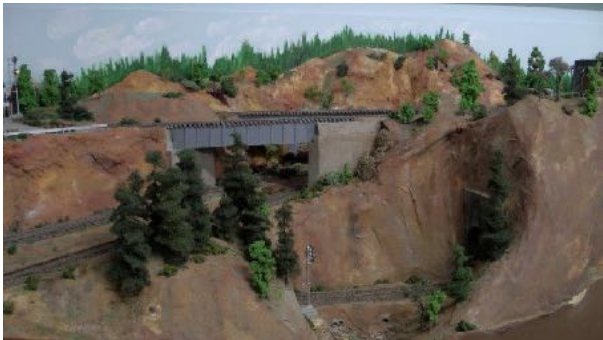
Michael Robinson told us he now has continuous running on his layout. He recently attended the BRMA convention and many conventioners took the opportunity to have a look at Michael's layout.

Jane Robinson has been assisting Michael with the library update. Jane has now contracted with a landscaper for their garden railway.

That concluded the meeting and the group then enjoyed afternoon tea and had a look at Ray Brownbill's layout. Our next meeting will be November 12th at Ron Solly's, 9 Grey Cres, Evanston Gardens.....

Photos:





Division 7

John Arrowsmith (NMRA Inc.-AR Div7 Superintendent)

NG-SIG Report by Steve Chapman, MMR®

August meeting:-

NMRA Division 7 Narrow Gauge SIG held a meeting at Jim Poole's residence. We inspected his fine Narrow gauge layout and found it ran beautifully.

Eleven keen modellers showed up to enjoy Jim's layout, as well as the stories getting bigger all the time.



Two pics of Jims layout, a beautiful HOn3 Trestle and a turntable




Paul Marrant showed us his new mill being built, again plenty of detail should look great when finished



Greg Hiley brought along his scratch built flat tops all scratch built. Again plenty of detail to look at.

Much discussion was had on the merits of turntables, all seem to agree that this part of the hobby needs to be improved.

The quality of models is really improving, due to the interaction between the members of the NMRA Narrow gauge SIG.

That's about it, the meeting broke up at about 4pm. The next meeting will be at my residence in November.....

Division 8 / 9 Northern Rivers

Ian West (NMRA Inc.-AR Div 8/9 Northern NSW Superintendent)

Regarding September / October meetings:-

There has been no report received for the Sept / October period from Division 8/9....

Division 10

Pat Britton (NMRA Inc.-AR Div10 Superintendent)

Regarding September / October meetings:-

There has been no report received for the Sept / October period from Division 10....

100% NMRA Inc.-AR Club News

Adelaide Model Railroaders Inc.

A 100% NMRA Club in Division 6

Club News - Running Night **October** 2022

By Ken House (AMR Newsletter Editor)

<https://adelaidemodelrailroaders.com>

We have a new member, Mark Whibley. Welcome aboard Mark. We hope that you will enjoy many happy hours running trains on the Southern Central rail road with us. As well as enjoying the AMR in general. Warwick Graham introduced Mark to the AMR. Mark was not previously a model railroader. He is one of Warwick's yachting mates who Warwick thought would enjoy our operating sessions.

So in a way our recently introduced operating sessions have resulted in an increase of membership. We should try to build on this.

OCTOBER SOUTHERN CENTRAL RR OPERATING SESSION

We were pleased to have eight operators arrive keen to operate on Wednesday night October 12. Eight operators meant we could run a complete crew given that each individual crew was a one person crew. The line up was five road crews, Peter Kirkland, John Prattis, Christiaan Werk, Mark Whibley, Paul Wright, Houseman Yard Master - Tony Mikolaj, Kingston Yard Master - Warwick Graham, Dispatcher/Train Master - Ken House.

On the previous Wednesday, Tony Mikolaj, Peter Kirkland, Paul Wright, and Ken House cleaned the wheels on all the locos to be run and cleaned the track. This paid dividends. All trains locos ran well making operating that bit more enjoyable.

We did have a few derailments. A couple of trains had cars derail as they departed Inge bridge (the swing bridge) just as they arrived in the Jeremy Junction passing loop. This brought about a bit of pressure because Christiaan Werk had the Houseman turn east arriving at Jeremy Junction while Mark Whibley had entered the block heading up to Jeremy Junction behind him with the Reefer Express east, and John Prattis was in the block between Phillips and Jeremy Junction heading west with the West Wind passenger express. Three trains heading for Jeremy Junction, one of which was derailed at the western entrance to the Jeremy Junction loop. The derailed Houseman turn held up proceedings somewhat, but then the Reefer Express derailed at the same spot complicating things even more. That was the worst stuff up for the evening so in reality they session went pretty well.

There was at least one missed set out due to not reading the switch list fully.

During the session I would like all operators to imagine them selves actually driving the trains. Turn locos where there are turntables. We have a lot of single ended diesels, and the steamer's crews do not like running tender first. Also please do not back into the Ippinitchie creek branch. Try to imagine that it is over ten miles from Opie to Ippinitchie creek not just a short hidden staging track. The Ippinitchie creek turn needs

to turn its loco at Opie and put the caboose on the other end of the train before it crosses the trestle bridge and goes into the tunnel (hidden staging). Having arrived at Opie from Houseman as a west bound and then departing east along the Ippinitchie creek branch. There is a track directly from Opie onto the turntable nearby.

It is still a learning curve for all of us and no one expects perfection. Which will be rarely achieved I believe. Even in real life crews sometimes missed set outs and pick ups. But they would not have backed up for a whole block of track to correct the error. The missed set out or pick up would have to be done on the return journey, or by another train resulting, possibly, in a customer complaint about late delivery.

At 8.55 the booster supplying power to the blocks Zieglersdorf to Atkins burnt out. Fortunately we had only twenty minutes to go. We do not have a spare so Kent Henschke has volunteered to purchase three 5amp NCE boosters for the club. Then we will have spares.

Had the booster burn out not occurred we would have almost finished the full sequence. This time we have done it in two sessions, mainly because we have had five road crew both sessions. Last sequence we only had four road crew for sessions one and three. Getting so close to finishing the sequence means that the best thing to do will be to re-stage completely before the next session and begin a new sequence next session.

Apart from the early ending for most crews, due to not being able to cross the blocks of track between Atkins and Zieglersdorf, the session was a success and we all enjoyed themselves.

The break down gave us more time to enjoy a cuppa, a cookie and some chat before heading home.

Thanks to all who came. Cheers Ken.

Right: John Prattis drove the coal drag from Redman mining co at Dent to Northern Power at Haynes. Locos for the coal drags are stabled in the round house at Kingston and travel light engine to Dent where they pick up loaded coal hoppers from



Redman mining co. Above SD24 number 240 is leaving the Kingston round house.



Left: Alco FA no 461 approaching Gayler Junction is actually on the return loop. The train is the TOFC train west which will pick up a cut of five TOFC from Rollanoff Transport at Houseman. The yard master will do the switching. We could do with up to six more flat cars suitable for TOFC to fill out this train with more TOFC.

Right: Unicorn timber and mining co RS1 number 1000 has just worked Prattis yard and is departing for Kingston with the Kingston turn east, where it will set out its train and pick up another consist of cars before heading back west to Houseman.



Left: Following a block behind the Kingston turn east was the Houseman turn east on its journey back to its origin, Kingston. Here Houseman turn's power GP7, Ippinitchie creek number 923, is setting out two flats to be loaded by Wright stone cutting at Werkendam.

Right: At Lower Kerry (Upper Kerry is the high level track above) the Southern Belle east is powered by an Alco PA.





Left: Meanwhile the Houseman turn east has moved on to Prattis where it is setting out a cut of three cars. The reefer behind the loco was picked up from Henschke Wurst haus at Wekendam and is in front of the Prattis cars. The reefer will stay coupled to 923 for the rest of the journey to Kingston where it will be set out at Jefferson abattoir.

Right: On the left, John Prattis train has arrived at Dent with empties from Northern Power and is setting out the empty hoppers at Redman mining co. On the right, Christiaan Werk is switching flat cars at the Opie saw mill. Christiaan's switch list can be seen hanging from a hook on the valance above.



Left: Having set out two empty flat cars at the Opie saw mill loading dock consolidation no 27, off the Ippinotchie creek turn, is retuning a loaded flat back ready to be picked up by a later train. The track in front

of the loco and flat car is the Ippinitchie creek branch line which goes over a trestle and then into a tunnel.

Right: Tony Mikolaj bought two reasonably priced mobile phone holders from a radio control hobby shop and adapted them for use on the SCRR. Warwick Graham Kingston yard master used this one to hold his phone.



WORKING ON THE SOUTHERN CENTRAL RR



Left: Tony Mikolaj is making progress on the tunnel on the lower track at the end of the peninsula between Atkins and Zieglerdsdorf.

Right: Truss bridge installed on the upper level mainline to the east of Prattis and a through girder bridge about to be installed in the lower main east of Atkins. Tony Milolaj is doing the work. The AMR recently bought the girder bridge from John Prattis. The truss bridge has been in our stock for many years.



OPERATING SESSION VIDEO



Left: Click on image to view a surprise video. This factory at Kingston is a scratch build. I don't know what it manufactures but it looks good. It was probably built by the late Len Opie.....[M](#)

Division 3

Peter Kendall (NMRA Inc.-AR Division 3 Superintendent)

Report provided by Rod Hutchinson

October 9th Meeting:-

Meeting Attendance and Apologies:

15 members


Meeting Report:

15 Modellers, Partners & Guests attended the October meeting held in the rural town of Skipton, located about a one hour west of Ballarat. Bob & Myra Thornton hosted the event at their home. The Thornton's backyard is big enough to cater for the largest of gatherings. Combined with Myra's culinary skills, guests enjoyed a sumptuous repast on a cloudy but dry afternoon, in contrast to the rainy day at the Bathurst car races.

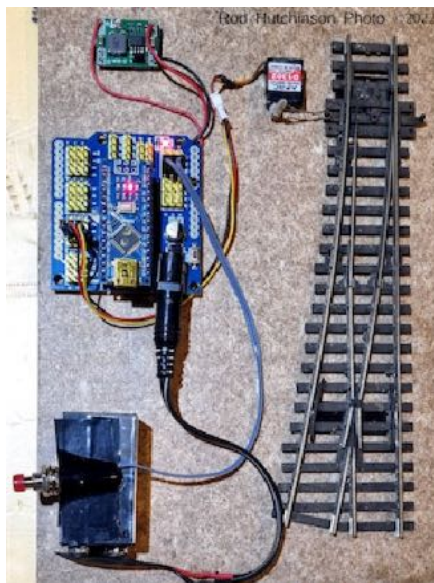
Guest came from as far away as Mooroolbark, E of Melbourne and Warrnambool on the SW coast of Victoria.

Bob has a mixed interest in narrow gauge railways, mostly of UK heritage. Most of his 16mm scale garden gauge was on display and a very old Bing 0-4-0.

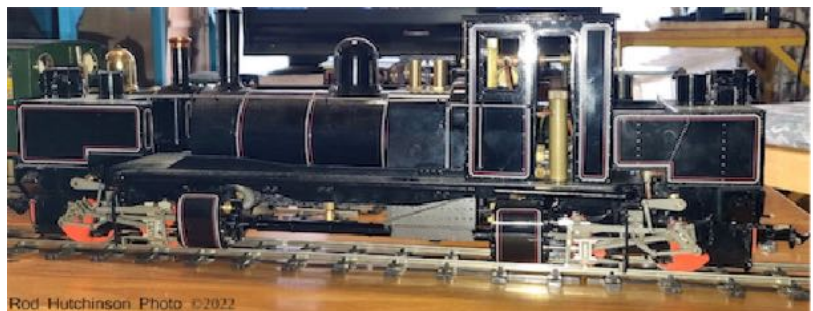
Much of the afternoon was spent discussing all things prototype and model railway. Rod Hutchinson provided a demonstration of his first Arduino Micro-controller Sketch (Code) operating a servo across two positions, with slow movement and single button operation. Code is good for 6 servos and maybe used for points and/or signals.

Peter Kendall presented our host Myra Thornton (she did all the work) with the NMRA thank you plaque.....

Models on display:



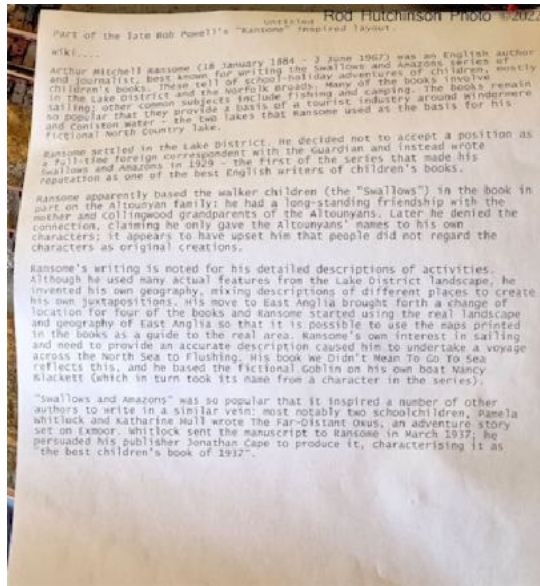
Above: Rod Hutchinson: Arduino Controlled, Slow Point Throw



Above: Bob Thornton: 16mm scale NG Beyer Garratt K1.

Below: Bob Thornton: 7mm-Bing 0-4-0.





*Above & Right: Bob Thornton:
Robert Powell(dec) O-16.5
layout.*



Above: Paul Richie: HO Assorted Scenery Items.



Members attending the October meeting.

Video at: <https://youtu.be/maA67ypF1Tw>



Division 4

From Frank Godde, MMR® (NMRA Inc.-AR Div4 Superintendent)

October Meeting:-

Meeting Attendance and Apologies:

7 members

4 guests

1 apology

AP Awards:

Nil

ARC Report:

Nil

NMRA Regional Feedback:

Nil

NMRA Division 4 Feedback:

We again discussed what we were going to plan for the 2023 AMRA show and Dennis will email a plan for the Expression of Interest.

Meeting Report:

The October meeting started off at the Swan View Tavern for lunch at 12pm, with eleven of us there. It was suggested that because it was our first outing for a long time that members bring their wives along too.

During lunch it was agreed that we should do this more often and in the next year an appointment will be made, probably for some time during winter. The food at the tavern was very good and at a reasonable price. We then drove up the hill to the Swan View tunnel and walked .7 of a kilometre to the entrance, where photos were taken to prove that we were there.



Alan mentioned that this tunnel did have a history of death as when the MSA Garrets went through there was only an inch clearance between the cab roof and the tunnel ceiling, therefore not giving much room for the smoke to dissipate and the driver and fireman were asphyxiated and the Garrets were banned from use especially at that slow pace and going up a 4% grade.

Peter was taken home before the walk as it was too hard for him but he enjoyed the


lunch. The tunnel floor was covered in ballast and puddles of water and on occasion you could see the steel rail still there. The key stone had the date inscribed in it saying



1895. The tunnel which took a year to build was blasted through solid granite and was started while John Forrest was the WA Premier and had the famous C Y O' Connor as its 'Engineer in Chief', with William Hedges as the tunnel engineer.

The tunnel was lined with the blasted granite rock up to 2.4 metres high and then the rest of the tunnel was lined with brick over the curve all

the way to the end. The tunnel is some 340 metres long. There was water running through the ballast but it caused no problems. We all enjoyed the walk and we covered about three kilometers.

When we as a family came from the migrant camp in Bathurst to Western Australia, we came through this tunnel, but now there is a bypass built around to take modern traffic.....



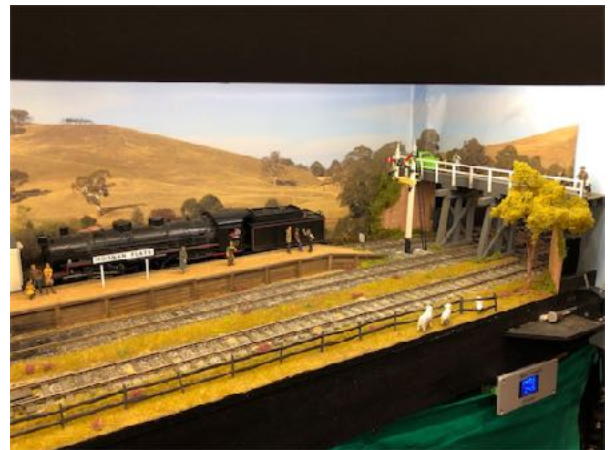
Armidale Convention - 2022

by Merv Bagnall

I must mention and give high praise to the organisers of the Armidale Convention, which was held over the weekend of the 22nd & 23rd October. The wet weather and cold conditions didn't detract from the event at all, and although it was not an NMRA-AR event, it was well supported and I am confident that NMRA-AR members would have outnumbered non members.

It was the first time that I have attended this convention, but it certainly won't be the last time. It was well run, had an excellent partners program, plenty of clinics, layouts and trade stands for us modellers to add to our knowledge and to pick up a bargain or two!

Here are a couple of happy snaps for the people who couldn't make it to the event.





What's in the Next Edition

- *Brent Wood has a fascination with the history of old dilapidated buildings, and likes to build models of them where possible. Brent has blended a very nice diorama from two photos of an early 19th century Country Store and Logging Camp that depicts such scenes.*
- *Jeff Lee, MMR® will complete the third part of his excellent three part series titled 'Building a Stub Ended Terminal', focussing on the Structures and Scenery needed to complete this new module.*
- *Would you like to know an easy way to power the frogs in your turnouts and provide direction indication and possible signalling? Then Erik Bennett's Article titled 'Frog Picker' may give you the answers you have been looking for.*

plus a lot more reading as well.....