



THE FLIMSY

NMRA Division 2 Newsletter

March-April 2018

From the Editor

Greetings.

One of the best things about attending a local NMRA meeting, besides the afternoon tea, is hearing about a modelling technique or some prototype information that really inspires. At the April meeting, we had both!

Ross Balderson showed us some exquisite N scale pointwork he had hand-built for his *Newcastle 1899* layout. Hear more about it in the *Show and Tell* section.

And Wal Pywell gave a fascinating presentation on the *Canadian Pacific Railway* which I summarise on the next page. And let's not forget the afternoon tea...

Brad

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Ross Balderson gave us a detailed overview of how he built the complicated crossover shown here (Photo: Ross B.).



April 2018 meeting

The April meeting was held at the home of Wal Pywell. There were fourteen members in attendance. Div. 2 Superintendent Stephe Jitts couldn't make the meeting and sent his apologies so it was left to John Gillies to chair the meeting.

John informed the group that meeting plaques would be given to all those hosts of previous meetings who had not received one. Please let John know if you are one of those people. John went on to report some NMRA business from the AGM on 10th March. The new committee was established and all the information about the elections and office-holders are in the latest issue of [Mainline](#). Suffice to say, the new President is Graham Young from Queensland. David North from Sydney was elected as Vice President. Former President David O'Hearn takes up the Secretary position while John Gillies remains as Treasurer. Stephe Jitts was appointed as the new Public Officer.

The [2018 NMRA Australasian Convention](#) is coming up in September and members should check out the NMRA website for convention information, including registration which is now open. At this stage there is still no host for the 2019 Convention but there is time for other Divisions to put their name forward.

Show and Tell (reported separately) was followed by a great presentation by Wal on the *Canadian Pacific Railway*.

On July 1, 1867 four provinces joined together to form the new country of Canada. The four provinces – Nova Scotia, New Brunswick, Quebec and Ontario – were joined three years later by Manitoba and the Northwest Territories. John Alexander Macdonald was a key driver of Federation and became Canada's first prime minister (1867-1873). In 1871 British Columbia decided to join Canada, but only if the Canadian government promised to build a transcontinental railway. After election defeat in 1873, Macdonald won again in 1878 and that's when the construction of the Canadian transcontinental railway started in earnest. One huge challenge was the crossing of the Selkirk mountains. Major A.B. Rogers, a surveyor, started looking for a possible route in 1881. It took him two seasons to find a pass that the railway could use to cross the Selkirk Mountains. The pass was called Rogers Pass in his honour. On November 7, 1885, Donald A. Smith drove the last spike into the railway tracks at Craigellachie, BC. Sir John A. Macdonald's dream of a transcontinental railway was now real. Eight months later the first transcontinental train left Toronto and Montreal, on June 28, 1886, for the Pacific Coast. Below photo by W. Pywell.

The afternoon ended with a terrific afternoon tea on the back porch. Thank you very much for a great day.



Show and tell

Ross Balderson started proceedings with some exquisite hand-made N scale pointwork for his *Newcastle 1899* layout. Naturally, we were all very impressed and wanted to know all about it. A full description is on the next page with photo. Ross did say that he likes to get into the “hardest things first” because it “can’t get any worse after that”.

Ross also showed us some scale drawings for a signal box in preparation for construction (windows to be etched in brass). Ross is investigating the *Megapoints* control for signal and turnouts, based on some information in the UK model railway press.

Jess brought along a cordless soldering iron fueled by butane that she considered good value. The [Superpro 125](#) was purchased from [Element 14](#). Jess also had some detail items - [O scale cats for windows](#) by [Vector Cut](#). They were available from *Railcar* but the shop is closing down.

Robin Foster alerted us to the latest catalogue from *Jaycar* - worth a look.

Jack Child showed us his scratchbuilt HO scale road bridge he is working on. The bridge is based on the one at Woolbrook. It is made from wood - the tension rods are still to be added. More information and a nice photo is on page 5.

John Bullen had three new book purchases to show us (see photo below). The books were in German (sehr gut) about the Swiss Railways.

Wal showed us some plug and sockets of different sizes for use with Arduinos.

Before seguing into Wal’s presentation, Peter Dinham announced he will be building a new layout when he moves house. He will have less space and will have some thinking to do about the best design. There was plenty of helpful (?) suggestions thrown his way and I am sure that the collective wisdom of the group should be able to work with Peter as he develops his plan for the new layout. We wish him well with that.

John Bullen’s German language books on the Swiss Railways (Photo: B. Hinton)



Show and tell (continued)

Ross explained how he went about the construction process for his pointwork.

The complex point and crossover section of Newcastle was drawn to scale using Corel Draw. Corel Draw is a software program. The drawing was then printed out on A3 paper.

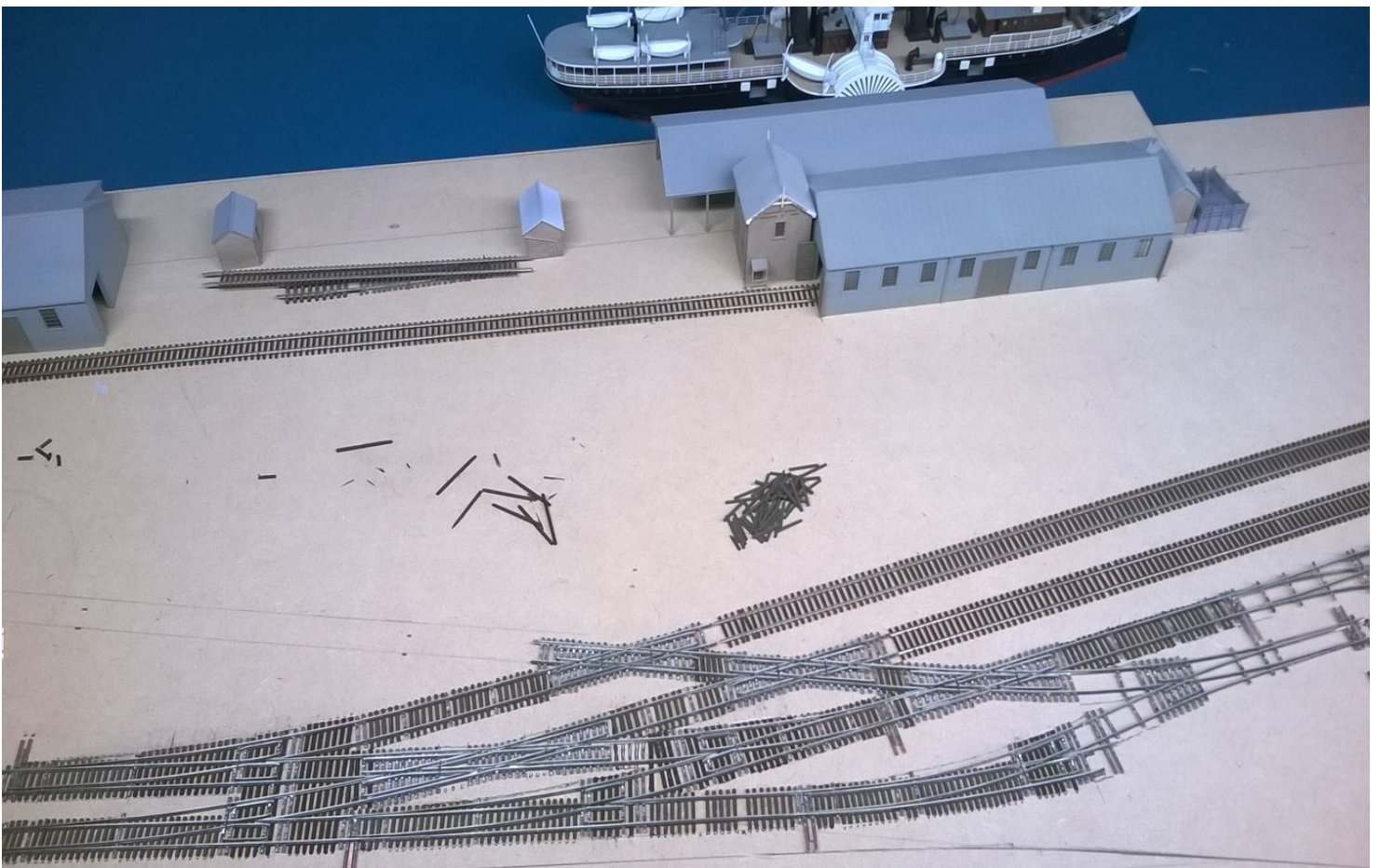
Copper circuit board sleepers were then glued in place onto the printed plan with PVA glue. Ross uses Micro Engineering code 55 rail. Once set, code 55 rails were cut and filed to length. The frogs and point blades were created using *Fast Tracks* filing jigs. The rail was soldered to the copper sleepers while using a NMRA track gauge.

A number of frogs were not a standard size so they were ground to the correct shape using a grinding disk on the Dremel drill. A small steel ruler was also used to align the rails on the crossovers.

Once complete, the copper-top coating on the sleepers was cut to electrically separate the rails.

And this is how it looks!

The complex pointwork in situ on Ross Balderson's under-construction *Newcastle 1899* layout (Photo: Ross Balderson).



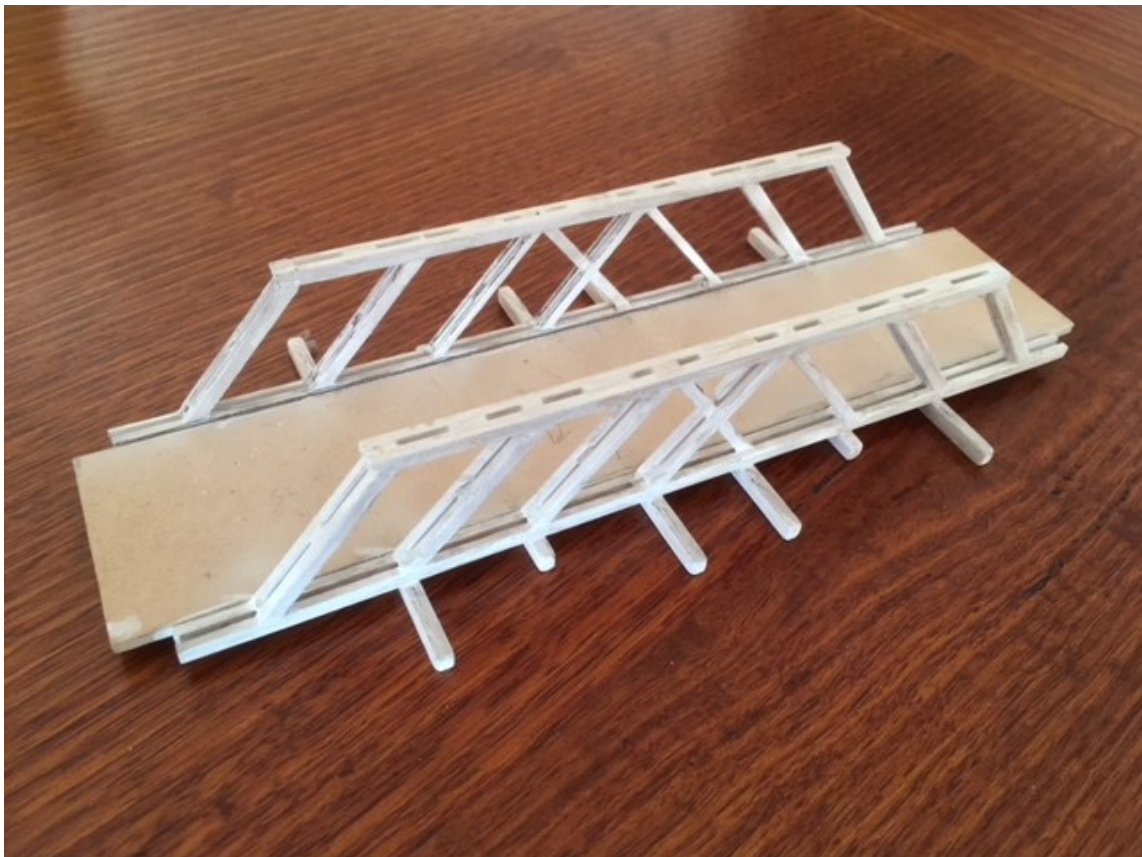
Show and tell (continued)

Jack Child brought along his under-construction HO scale road bridge. The bridge is an approximation of the road bridge at Woolbrook, NSW, and has been constructed from basswood strips. Construction is based on *Greg Edwards Data sheet P14*, and reference photos taken in 2012 and 2017. Since the data sheets are to scale, it's easy to use them as patterns for construction.

The Data sheet shows a rail bridge, and differs from the road bridge in several ways. The rail bridge has double beams underneath while the road bridge uses only single beams. While at first glance the two bridges appear quite similar, they also differ in the size of the beams, the number of cross braces and the positioning of the tension rods.

The Howe Truss differs from the Pratt Truss in that the tension rods and braces are interchanged. The Howe Truss uses vertical tension rods, the Truss uses diagonal tension rods. With the Howe Truss the diagonal braces are in compression, while the Pratt Truss uses vertical beams for the compression element. Jack's bridge will be a fine scenic item on his under-construction layout.

The photo shows Jack's road bridge that he is building using basswood strips (Photo: Jack Child).



The last say....

In just over a week I will be flying out to the United States for a ten day trip to Midwest America. I will be checking out some more prototype rail-road action and infrastructure along the Dakota, Minnesota and Eastern Railroad (DME); Canadian Pacific since 2007.

But the main reason for my trip is to attend the *Thousand Lakes Region NMRA Convention* in Sioux Falls, South Dakota. I will be a presenter with a talk on model railways in Australia. Apologies to Matt in advance of his hosting our local NMRA group on 19 May.

I can safely say that the best part of these conventions is the friendships you make along the way. One of my best friends is a chap I met in Sioux Falls in 2011 on my first trip to the USA. Alan has been a huge influence on me and I really appreciate his advice and friendship. He now lives in Kansas City but it will be great to catch up with him again in Sioux Falls.

I know there are many of you who have had similar experiences. It is one of the great benefits of this wonderful hobby of ours.

Brad

The next meeting

The next meeting will be down the coast at the home of Matt Semanas at Long Beach - Saturday 19 May with a 1pm start. Details to come.

NMRA Australasian Regional Convention

The 2018 Convention is on 21-25 September at the Helensvale Community Centre and Library. It's been held there before and is a great venue. There will be the usual clinics and layout tours. Keep checking [the website](#) for updates.

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What and where is this locomotive?

