



THE FLIMSY

NMRA Division 2 Newsletter

January 2019

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From the Editor.

G'day and welcome to the New Year 2019.

The January meeting at Mal Risby's residence was well attended with a veritable swag of show-n-tell items as information on topics on updates, general interest to current events within our area.

Stephe opened the meeting by welcoming a new member to Division 2 Terry CRAIG.

Where to start: as one topic in particular requires more than just a gloss over was that Stephe presented discussion from Al Harris Division 8 Superintendent NMRA-AR who noted in an email [which has been edited as appropriate Ed.] "At Malkara in 2016 I was asked to construct a small switching layout for an Autistic boy and then set about constructing the layout in time for Christmas. Thus was born NT Junction which is a play on his initials N*** T***. Having the layout has been an asset to the recruiting drive conducted in Sydney over the past few years. Now I want to pass it on to N*** so that he can get some real enjoyment from it."

"During discussions with B*** at Malkara in 2016 she advised me that Noah had operated 'The Bottoms' in 2015 and had brought him back in 2016 to have another drive of the layout as he is mad keen on model trains. I've since learnt that many sufferers of autism have an affinity for model trains. At our Coffs Open Weekend I spoke with the mother of a 2 & 1/2 year old boy who was also keen on model trains. She explained how he had learnt to drive his wooden train around its wooden tracks and was interested in getting him a Thomas set after talking with Trainman, Paul Baker, on this matter."

The Question: Is this an area of model trains that the NMRA would want to explore?

Proposals to the above were opened for discussion at the meeting.

Note: N*** T*** & B*** real names suppressed for privacy.

Keep on training.

Robin.

January Meeting

John Gillies presented an update to the late Geoff Nott and Michael Flack built and exhibited the well-known Smuggler's Cove layout http://www.nmra.org.au/Layout_Tours/Smuggler%20Cove/index.htm and <https://www.youtube.com/watch?v=JdTb8XBR9ss> which is going to be incorporated into the permanent "Magic of Scale Model Railroading" exhibit at the California State Railroad Museum in Sacramento, California.

As reported in the most recent NMRA eBulletin, there have been some delays in progressing this initiative. NMRA AR selected Grace Removals after a competitive quotation process to ship the layout to California, but delays in the USA with the contractor building the exhibit have resulted in the cost of shipping increasing from \$5,880 to \$6,605 since the original quote was received in August last year.

The layout was collected on 29 January and is on its way to California; The "Magic of Scale Model Railroading" exhibit was previously scheduled to open late in the 2019 northern spring. I don't know if the opening date has slipped as a result of the delays in the USA.

John Bullen brought us up to date with what used to be the ACT Division of the Australian Railway Historical Society. In short, the ARHS no longer has an ACT Division. In its place has arisen a new and completely independent organisation, namely the Capital Region Heritage Rail Ltd. This company now runs the Canberra Railway Museum.

To repeat some painful history, the ARHS ACT Division collapsed in financial ruin more than two years ago, facing debts between half a million and one million dollars. A liquidator took over and has recently completed his duties. Much rolling stock and other assets had to be sold off to pay creditors.

Recently restored to full running order, the huge Garratt loco 6029 is now with Transport Heritage NSW, periodically undertaking special tours.

The historic 1210 loco remains in Canberra because it is the property of the ACT government. It is not currently in running order, being under repair at the time of the financial collapse. Since then, some important parts have been stolen so the future of 1210 as an operating loco remains in doubt.

Prior to 2016, the ARHS ACT Division undertook a wide range of activities, such as...

...Short distance railway tours, mainly to Bungendore, by steam or rail motor.

...Long distance railway tours, mostly diesel powered and within NSW.

...Railway tours to attend special events, mainly in Sydney.

...Restoration of historic rolling stock.

...Steam locomotive maintenance.

...Running commercial railway operations, mostly scrap metal.

...Managing the Queanbeyan ticket office for Country link.

...Managing the Canberra Railway Museum, in Kingston.

With very limited funds and rolling stock, the only currently feasible operation for the new organisation is to manage the Canberra Railway Museum. It is hoped to expand operations further in due course.

For many years the ACT Government has sought to take over the Kingston site for redevelopment; however the Canberra Railway Museum is clear to remain there for at least the next five years.

The new organization has been set up by John Cheeseman and a few others. Those involved have steadfastly declined to probe the financial disaster, presumably to avoid embarrassing those responsible for the inept (though well intended) previous management.

Former members of the ARHS ACT Division were kept up to date with regular email bulletins until mid-2018 when the new Capital Region Heritage Rail Ltd officially started. At that point the former ARHS members were invited to pay their membership fee to join the new organisation. Further news bulletins would only be sent to those who joined up. It is believed that the current membership strength is about a quarter of the previous membership.

For further information see www.canberrarail.org.au

Wal Pywell sort answers on insulated vs electro frogs, types of motors to power & micro switches which proved an avenue for continued discussions.

Stephe Jitts Stephe briefly described his journey from Cairns to Yass. It involved a 25hr "Sunlander" run in a Sleeper which he described as being very similar to that available in first class air travel today. This was followed by a 3hr bus trip from Brisbane to Casino and a 12hr run in a NSW XPT sleeper. The contrast between the Queensland and NSW sleepers was marked with the only advantage in the NSW trains being greater privacy. A 20m walk across the platform to join the Melbourne bound XPT (this time just 3 ½ hrs in Economy) had him home before midday. The whole trip took just two days two hours."

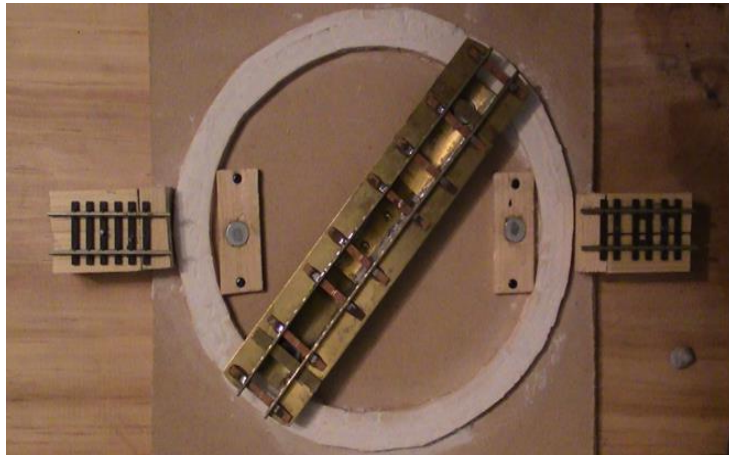
Show-n-tell

Rob Nesbitt continues with his modelling of the abandoned line from Wagga Wagga to Tumbarumba. Here is a 1.2 meter diorama of the 2 bridges across Kyeamba Creek. I was not able to get a photo at the meeting, but here are ones Rob provided more can be seen on Rob's blog 'Building Wagga' <http://buildingwagga.blogspot.com/>





The other was a work in progress of the 60 foot turntable at Tumbarumba. Rob had used a computer hard drive bearing as the pivot and some magnets for alignment of the tracks.



After the show-n-tell the next level of construction, pictured above, supplied by Rob.

David Low presented two kits both in 1:35 scale, one a German diesel shunter called a WR 360 C12. This kit is a Trumpeter kit No 00216

Trumpeter continue their locomotive/rolling stock series of kits with this release of the German WR 360 C12 Locomotive of which only 10 were produced and were never used outside the borders of Germany being used mostly on shunting duties in rail yards and similar mundane tasks during their lifetimes which for some extended into the 1960's.

The V36.0/WR 360 C12 was a diesel powered locomotive of modest size being only around 9 meters in length and was superseded by the slightly larger and more powerful V36.2-4/WR C14 which was built in larger numbers and used extensively during and after WWII with some still operating as late as 1974.



The other kit being a THUNDER German Gedecker Guterwagen G10



Jack Child presented his near completed Walcha Road Hotel

The Walcha Road Hotel was originally opened in 1860. The hotel was nearly destroyed by fire in 2004, but was eventually restored, and re-opened in 2007.

In more recent times it was known as the Royal Hotel, but has since been re-badged again as Walcha Road Hotel, following its re-opening.

The model of Walcha Road Hotel was scratch-built using mdf for the basic building. Exterior walls were covered with brick paper, while windows and doors were acquired from Tichy. The chimneys were also a purchased item. The roofing was fabricated from small aluminium foil trays which were corrugated using wood siding as a template. Square brass tubing was used for the veranda posts, thinking it would be good if this was really strong! A single LED porch light provides visibility into the veranda.

Appropriate signage was located on the internet, and then printed to scale for the front of the building. The men's toilet sign is a printed reduction of the actual sign.



The sign cut from their business card, just the downpipes to be added.



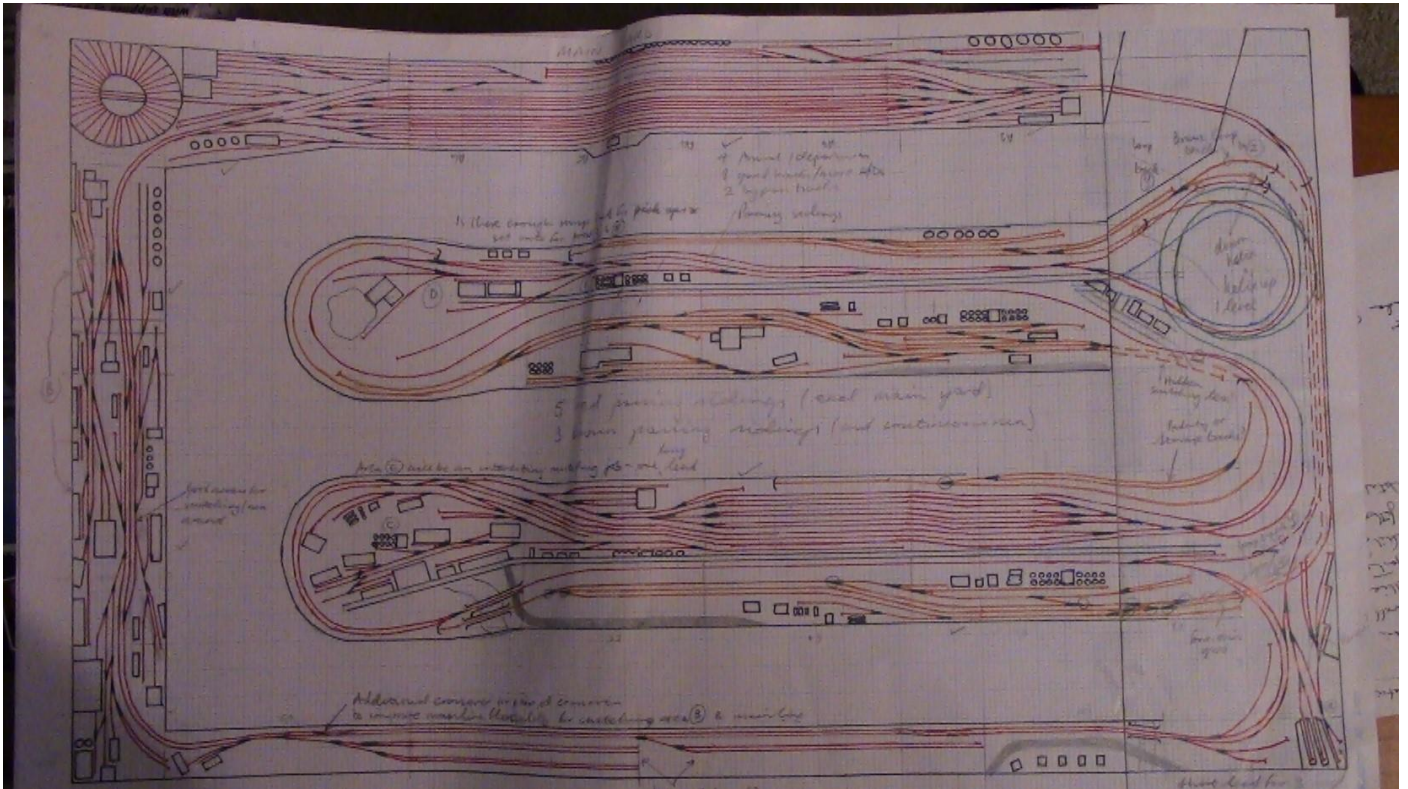
Robin Foster mock-up of two O scale LED lights [purchased off eBay as Christmas decoration Lights for some bench top lighting operating which work on 3 Volts, more will be added at some stage.



Below: Couldn't resist this bargain off eBay mostly badger needles & a smattering of adaptor connectors.

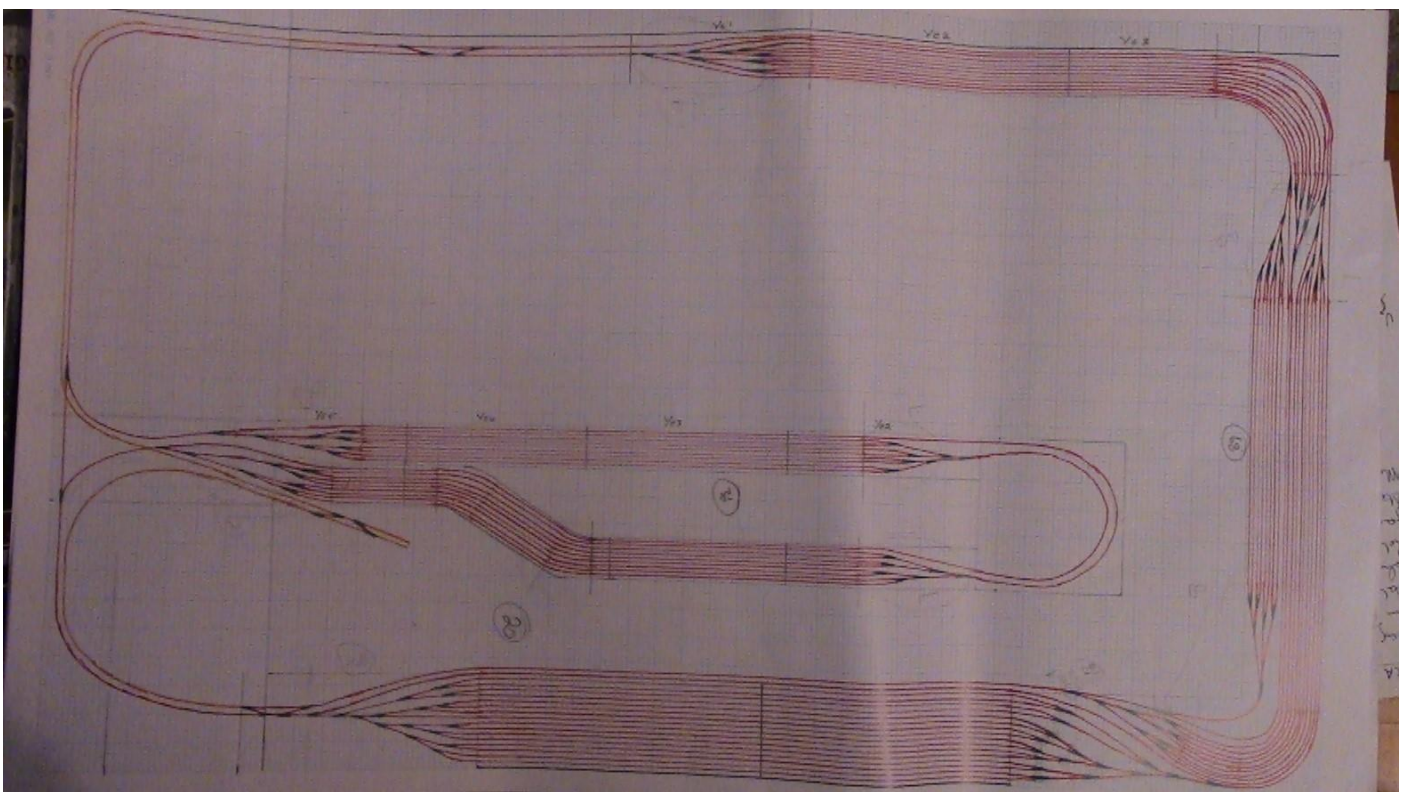


Mal Risby explained the latest workings of progression to his N scale layout passing around pages of plans for the lower & upper levels with multiple loops with his staging yard in the latest progress to laying one of the staging yards so photographed in the layout room in full page.

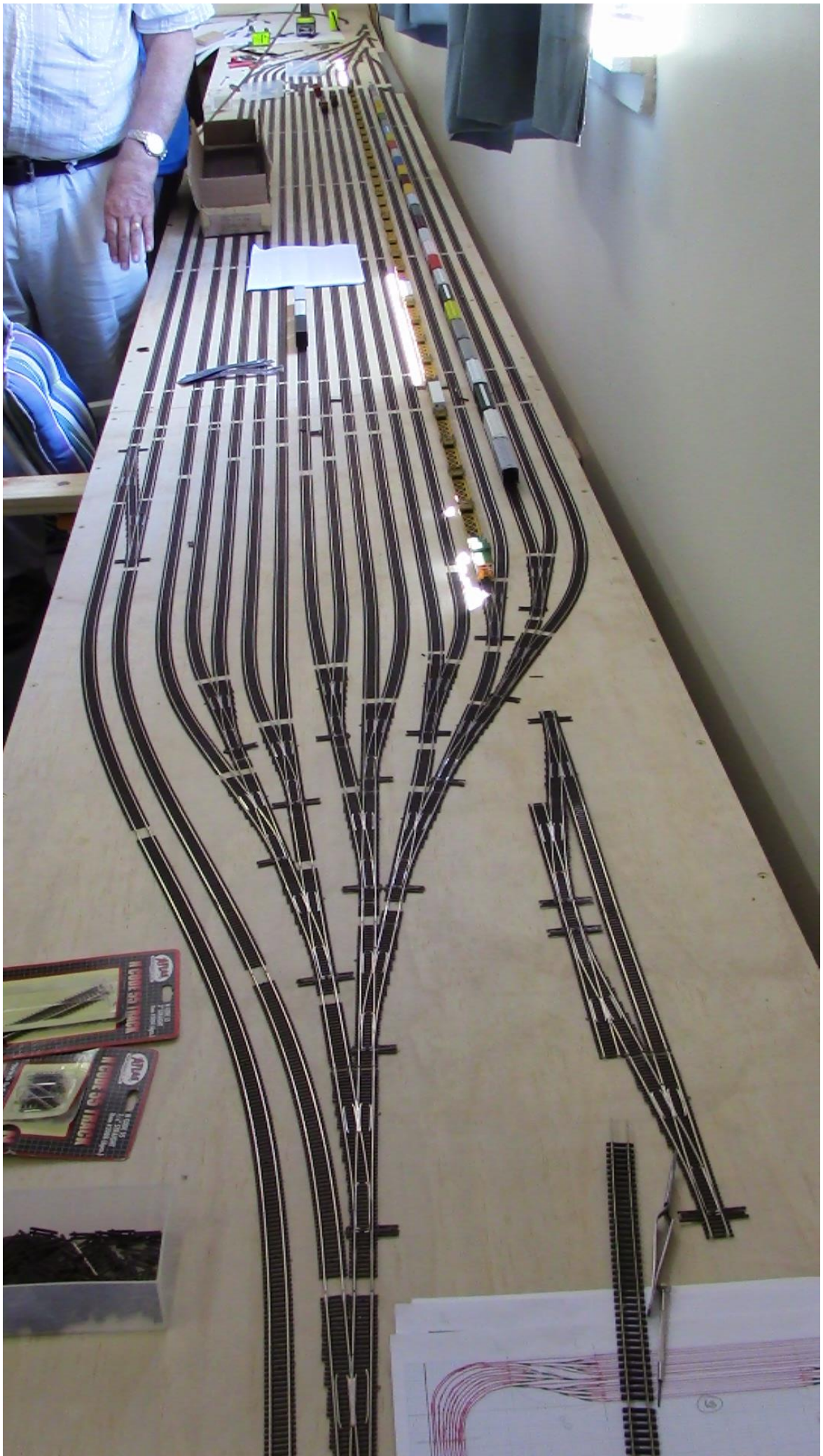


Above: The red colour indicates the upper level and orange Sub level

Below: Plan of the lower level



Mal provided an update on information with the discussions with the manufacturer to allow 400 addresses that are required to operate 3 to 4 locomotives lash-ups for his Digitrax system. Additional information to Mal's layout can be viewed in the January 2018 article in The FLIMSY.



Mal also mentioned that he is planning to use JMRI Operations on his new layout and there was some discussion on this aspect of operations.

The January 2019 NMRA Turntable email included a link to a clinic in the members area of the NMRA USA website which was given at the 2018 Kansas City NMRA National Convention - "Operations and Car Forwarding" by Mark Juett which provides some information setting up and using JMRI Operations. After clicking the following link you have to login to your account to view the video of the clinic <https://www.nmra.org/clinics/operations-and-car-forwarding>.

It's also worth mentioning that there are many excellent and very useful clinics given at previous NMRA National Conventions on a huge range of useful and interesting topics available on the NMRA website. Just create your own member account by clicking the Member Log-In icon on the top right hand corner at <https://www.nmra.org/> (if you haven't already and register with your email address, member number and your nominated password) and away you go.

The last say.

2019 is another year in our calendar where much has been contributed to the advancement on many changes to modelling technologies over the decades, however for reasons that only can be explained to delays of projects from retailers from manufacturer's remains a challenging dilemma to plausible explanations for such 'overdue' delays where one has outlaid either an EOI / deposit, prepayment to obtain an incentive for a small saving to the RRP product release date or made full payment for those advertised item's for a projected expectant date.

Occasionally there have been cancellations to projects as quotas for Expressions of Interest [EOI] have not been reached or costing blowouts such as requirements in upgrading / finding those replacement parts, even during production, as the factory advises / amends costing & negotiations fail to accept those said increases.

There is also the 'overused' terminology phase 'CNY' at this time of year for production delays & deliveries.

Saying the above one needs to focus on ones or 'club' projects for completing the assigned task in the quest of enjoyment as fulfilments to the hobby, so in this case one must enter the tunnel to sight the light and then arrive to the other end of the tunnel to.....

Keep on training.

Robin.

The next meeting will be at Stephe JITTS residence Old Linton YASS start 1300 please RSVP to advise Stephe of attendance.

2019 if you are interested in hosting a meeting this year contact Stephe who will provide necessary information.

The FLIMSY contact robinfoster@iinet.net.au